



HISTORIC BEGINNINGS • 1847

**Farmington City Planning Commission**

**July 9, 2020**



# FARMINGTON CITY

H. JAMES TALBOT  
MAYOR

BRETT ANDERSON  
SHAWN BEUS  
SCOTT ISAACSON  
AMY SHUMWAY  
REBECCA WAYMENT  
CITY COUNCIL

SHANE PACE  
CITY MANAGER

## AGENDA PLANNING COMMISSION MEETING

**July 9, 2020**

Public Meeting at the Farmington City Hall, 160 S. Main Street, Farmington, Utah

Study Session: 6:00 p.m.

**Regular Session: 7:00 p.m.**

*Farmington City Planning Commission meetings, including this meeting, are open to the public. In consideration of the COVID-19 pandemic, if necessary, members of the public wishing to attend this meeting are encouraged to view the meeting online. In the event this occurs, the link to view the hearings live and to comment electronically can be found on the Farmington City website at [www.farmington.utah.gov](http://www.farmington.utah.gov). In-person attendance is also an alternative, but any in-person attendance/gathering will meet the latest governmental restrictions related to the COVID-19 virus. If you wish to email a comment for any of the listed public hearings, you may do so at [crowe@farmington.utah.gov](mailto:crowe@farmington.utah.gov).*

- 7:00 1. Minutes  
2. City Council Report

### SUBDIVISION

- 7:10 3. Guy Haskell/Updwell Development LLC – Applicant is requesting preliminary plat approval for the Sydney’s Corner Phase 2 Subdivision, consisting of 4 lots on 0.94 acres of property located on the southwest corner of 650 West and Glover Lane in an AE (Agriculture Estates) zone. (S-1-20)

### PROJECT MASTER PLAN/ZONE AMENDMENT APPLICATIONS

- 7:20 4. STACK Real Estate (Public Hearing) – Applicant is requesting a recommendation for approval for the North Farmington Station Project Master Plan (PMP) a mixed-use development, and accompanying development agreement, encompassing approximately 142.79 acres north of the vicinity of Burke Lane, west of I-15, east of the D.& R.G.W., and south of Haight Creek (PMP-2-20).
- 7:30 5. Farmington City (Public Hearing) – Applicant is requesting approval of zoning map amendments concerning the rezone of City-owned remnant rights of way. (ROW) (Z-8-20)

### CONDITIONAL USE PERMIT

- 7:40 6. Ryan Nielsen (Public Hearing) – Applicant is requesting conditional use approval for a detached garage located at 1774 West Spring Meadow Lane located on .26 acres of property, in an AE (Agriculture Estates) zone. (C-2-20)
- 7:50 7. JD Tyrell/Wasatch West Contracting (Public Hearing) – Applicant is requesting conditional use and site plan approval for Farmington Bay Storage Phase 3, as a self-storage facility on 3.51 acres, located at approximately 1272 S. 650 W. in an LM&B (Light Manufacturing and Business) zone. (SP-5-19)

### OTHER BUSINESS

- 8:00 8. Miscellaneous, correspondence, etc.
- a. John Saltzgeber/JMSRE Investments LLC (Public Hearing) – Applicant is requesting a special exception to add an additional lot on .21 acres of property, located at 1454 S. 200 E. in an R-2 (Multiple Family Residential) zone. (M-5-20)
  - b. July Schedule
  - c. Other

*Please Note: Planning Commission applications may be tabled by the Commission if: 1. Additional information is needed in order to take action on the item; OR 2. If the Planning Commission feels, there are unresolved issues that may need additional attention before the Commission is ready to make a motion. No agenda item will begin after 10:00 p.m. without a unanimous vote of the Commissioners. The Commission may carry over Agenda items, scheduled late in the evening and not heard to the next regularly scheduled meeting.*

Posted July 6, 2020

Meagan Booth  
City Planner

**FARMINGTON CITY  
PLANNING COMMISSION**

June 11, 2020

**ELECTRONIC AND IN PERSON MEETING**

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**STUDY SESSION**

**Present:** Chairman Roger Child, Vice Chairman Alex Leeman, Rulon Homer, Larry Steinhorst, Greg Wall, Russ Workman, and Alternate Commissioner Inger Erickson. **Staff:** Community Development Director David Petersen, City Planner Meagan Booth, Recording Secretary Carly Rowe, Planning/GIS Specialist Shannon Hansell, Assistant City Manager/Economic Development Director Brigham Mellor and City Manager Shane Pace. Mike Plaizier was excused.

**Item #5 Miscellaneous, Correspondence, Etc.**

**a. Open House Results**

- a. **Shannon Hansell** presented the feedback from the Open House that was on June 2<sup>nd</sup> to discuss how Commerce Drive and Digital Drive will go through the City, which can be viewed on the City's website.

**b. One lot development proposal**

- a. **John Saltzgeber** who owns property on 1470 South wanted the Planning Commission's input regarding a tri-plex on this property. The Planning Commission looked at proposed ideas and there were no concerns at this time. His next steps will be filing for a Plat Amendment and Special Exception for a possible moderate-income housing unit.

**c. July Schedule**

- a. **Carly Rowe** informed the Planning Commission that we would switch our single meeting from July 16 to hold two meetings; July 9 and Wednesday July 22. We should have at least 5-6 commissioners here on those dates therefore we will have a quorum. **Inger Erickson** will be excused from both meetings and **Larry Steinhorst** will be excused from July 22.

**Item #2 Farmington Station Parkway Preliminary Plat Approval**

**Meagan Booth** said this project previously had a number of townhomes and now reduced to 50 per City Council recommendation. The townhomes will come in separately on a site plan; tonight the review is primarily for Lot 1 and parcel A, and subdividing that lot. After the review from staff, it is recommended to not have the 20-foot trail as emergency/secondary access. The road will be constructed to the property line and the utilities will be extended as well. There will have a fire turnaround and public works will have access for a turnaround as well. We will have to modify the motion to have an exception for secondary access added (#6). The developer worked with the property owner and the crash gate is up in the air. COVID-19 did affect this development, there were previously going to be two hotels and an office building. At this time, that is future development. **Dave Petersen** said Brighton Homes has over 24 units and they had that secondary access on the hotel side. **Greg Wall** asked if the hotel will go in eventually, **Meagan Booth** said the use is allowed but the zoning designation does help.

**Item #3 and #4 Project Master Plan for STACK Real Estate Development Agreement and Rezone of 36.5 acres.**

**Dave Petersen** gave a preview of this item and the location near the freeway. They are looking to rezone three of the parcels from A to OMU, which is consistent with our Master Plan. They are also asking for flexibility via section 140 in the event if they need to deviate from the standards.

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**REGULAR SESSION**

**Present:** Chairman Roger Child, Vice Chairman Alex Leeman, Rulon Homer, Larry Steinhorst, Greg Wall, Russ Workman, and Alternate Commissioner Inger Erickson. **Staff:** Community Development Director David Petersen, City Planner

*Meagan Booth, Recording Secretary Carly Rowe, Planning/GIS Specialist Shannon Hansell, Assistant City Manager/Economic Development Director Brigham Mellor and City Manager Shane Pace. Mike Plaizier was excused.*

**Chairperson Roger Child opened the meeting at 7:03 PM.**

**Item #1 City Council Report**

The three items discussed at the last Planning Commission; Meadows at City Park Phase 3, Farmington Station II and Farmington Station Center gained approval via recommendation from the Planning Commission. The Community Development Department also had two plat amendments and two cash bonds that went before the City Council in a Summary Action list. Lastly, the Mountain Bike trail “The Farm” received the grant that was applied for.

**SUBDIVISION**

**Item #2 Taylor Spendlove/ Brighton Homes - Applicant is requesting preliminary plat approval for the Farmington Station Parkway Subdivision on 8.41 acres of property located west of Station Parkway and east of Shepard Creek. (S-21-19)**

Brighton Homes requests a subdivision of 8.41 acres of property, creating one lot, known as the Farmington Station Parkway Subdivision. The Planning Commission approved the Schematic Subdivision Plan on December 12, 2019 and the Schematic (Concept) Design plan on March 5, 2020. The developer is proposing 50 townhomes for this site, which is consistent with the recommended amount from the City Council. The subject parcel was zoned GMU (General Mixed Use) by the City Council on April 14, 2020. The northern remainder parcel, Parcel A, is zoned OMU (Office Mixed Use) and will be developed later. The OS (Open Space) zone next to Shepard Creek will remain. The applicant at this time is seeking approval to subdivide the property, which requires approval of the preliminary plat by the Planning Commission.

**Taylor Spendlove** (Brighton Homes) did not have anything to add to what staff presented. **Greg Wall** asked a question regarding the crash gate on this property; they have communicated with neighbors nearby and are still considering that as a possibility. There is an access easement on the west property boundary. In agreement with staff, the 20-foot trail would not work as an option. **Greg Wall** questioned what the timeframe is for a pioneer agreement; **Dave Petersen** replied that the times vary.

**MOTION**

**Rulon Homer** made a motion to move that the Planning Commission approve the Preliminary Plat and exception for the Farmington Station Parkway Subdivision subject to all applicable Farmington City development standards and ordinances and the following conditions:

1. The developer will need to dedicate the right of way and all utilities will need to be stubbed to the west property line.
2. The developer must address the need for secondary access through a special exception or other means.
3. The developer will need to construct the road to the west property line, which may include an agreement with the city.
4. The applicant must obtain a flood control permit from the county.
5. All outstanding DRC Comments must be addressed.
6. The City Council must approve an exception to the City dead-end street standards to allow the developer to construct up to 50 dwelling units on the east to west street as shown on the preliminary plat.

**Russ Workman** seconded the motion, which was unanimously approved.

Findings for Approval:

1. The preliminary plat is consistent with the schematic plan.
2. The motion is consistent with the goals and purposes of the General Plan and Zoning Ordinance, including, but not limited to, the City's regulating plan for the area.
3. The character of the site will be maintained as the OMU zoning designation ensures a greater mix of uses for the area, which is consistent with the General Plan.
4. The right of way is set and the open space boundary is established.

#### PROJECT MASTER PLAN/ZONE CHANGE APPLICATIONS

**Item(s) #3 STACK Real Estate (Public Hearing) – Applicant is requesting a recommendation for approval for the North Farmington Station East Project Master Plan (PMP), and accompanying development agreement, a mixed-use development encompassing approximately 104 acres next to the west side of I-15, north of Burke Lane, east of the general vicinity of 1525 West Street and north towards the Shepard Lane area (PMP-2-20)**

**#4 STACK Real Estate (Public Hearing) – Applicant is requesting a recommendation for zoning map approval of approximately 36.5 acres of property in the vicinity of Spring Creek between 1525 West and I-15 from A (Agriculture) to OMU (Office Mixed Use) and OS (Open Space) (Z-2-20)**

Agenda items 3 and 4 are inter-related and it is proposed that staff present these items together and the Planning Commission account for the same in one public hearing. Moreover, the background information in this staff report, the findings, and the supplementary information are all relevant to each item; therefore, it is further proposed that the Commission consider the two requests in separate motions, but that the same findings are established for each item.

**Russ Workman** removed himself from the vote and discussion due to personal interest in the proposed development.

In November of 2016, Chartwell Capital and the City contracted with Urban Design Associates (UDA) to conduct a planning charrette, which produced a conceptual master plan for the 220+ acres of property north of Shepard Creek, west of the UP tracks, east of the D&RGW trail, and south of Shepard Lane. The charrette process involved receiving input from a number of stakeholders, including 13 property owners within and adjacent to the project area, city staff, local elected officials and representatives from Chartwell Capital. The end result was a master plan document, or sub-area master plan to the City's General Plan, intended to guide and inform the development of a future mixed-use office park.

The above referenced applicant, STACK Real Estate, is now proposing a more specific Project Master Plan (PMP) encompassing some 104 + acres of the UDA master plan area for the reasons set forth in the findings below.

On Tuesday, June 2, 2020, Farmington City held an open house to receive citizen input regarding a proposed realignment to "Commerce Drive", which is the major north to South Street, intended to connect 950 North to Park Lane. The UDA plan shows the corridor for this principle street close to I-15 and the U.P. tracks. The proposed alignment is located further west at 1525 West. The North Farmington Station East PMP places the Commerce drive corridor in an alignment consistent with the information presented at the open house, and the enclosed PMP shows a new principle street, "Digital Drive" between "Commerce Drive" and I-15, which is consistent with plans displayed at the open house.

An issue remains regarding the development agreement---that is, the proposed office to residential ratio set forth in paragraph 5.b. of the enclosed development agreement. Although the applicant is in favor of such a ratio, he maintains that due to the present uncertain office market he cannot fulfill this commitment right now. This topic will be discussed in further detail at the meeting.

STACK Real Estate representatives **Andrew Bybee, Nathan Ricks and Trevor Evans** (2801 N Thanksgiving Way, Lehi UT) presented some previous work that they have done along the Wasatch front, indicating that they specialize in Class-A Urban buildings. **Andrew Bybee** mentioned that when they look for property to develop, they look for *SMART*

communities, which includes Sustainable, Mixed Use, Attractive, Realistic and Transit-oriented development. They also want to ensure that the office is appealing to both tenants and the community. In Farmington along I-15, they have a blank canvas and have the chance to get the area just right. Depending on the office use, they can fit 4 to 8 office buildings in this area.

**Greg Wall** asked under section 140, if there is any provision that would require any commercial uses before residential. **Andrew Bybee** replied, with the uncertainty right now it is hard to make the decision but their hope is that offices come back sooner rather than later. He goes on to answer that the market will decide which comes first. **Alex Leeman** adds on in paragraph b, there is a ratio for office to residential. He understands that there is a possibility that changes. **Greg Wall** also asked if the buildout is projected through 2049, and if there is any expectation to have that done beforehand. **Andrew Bybee** answered that if the market is right then there is a possibility. The infrastructure build is about 12-24 months out at this point per **Andrew Bybee** and ready to market at that time.

**Greg Wall** questioned private roads and what/if any will be dedicated to the City. **Brigham Mellor** answered with results from the Commerce Drive open house, he indicated how much of a need this infrastructure is, if this road is not here, those getting off the West Davis Corridor, and the Shepard Interchange will go through the residential roads. Residents are concerned about the high school students but once WDC is in, it will also be traffic for holiday season to Station Park and for the new development. Typically, the City relies on developers to put those roads in and then they turn them over to the City as a dedicated road. **Roger Child** asked what the top projects would be on this, **Brigham Mellor** noted Digital Drive (2-lane/1 each way with on street parking) and Commerce Drive (4-lane/2 each way with on street parking), with Commerce Drive having the top priority for the City. **Greg Wall** questioned within the SMART concept that STACK applies, what the “sustainable” aspect would be for this, **Andrew Bybee** broke it down to sustainable construction and/or development, which includes mixed use and the transit portions of SMART. If they build a community where people can live, work, and play – it reduces emissions and reduces traffic on the roads in most cases. It saves water, landscape, utilities, sewer and power as well. On the remote shuttle to connect to Frontrunner, **Greg Wall** asked if any research has happened for this. **Andrew Bybee** along with staff has done research to know it is possible but not set at this point.

**Inger Erickson** brought up the issues where offices are not being used as much and work-at-home is becoming more of a normality, she asked what can we do as a City to make sure enough research is done before development happens? **Alex Leeman** said this is an interesting issue at this time; we cannot base what is going on in today’s world, what will happen in 3-4 years. He said the City has invested and we have only so much open space left; we have to encourage the right options in these areas. **Andrew Bybee** also replied that they do not take this development lightly, and they were supposed to meet with us in March, prior to COVID-19. He goes on to say that, this is a great location with a transit hub and that this is an ideal location. They also look at the economic asset of the residents in the area. **Alex Leeman** replies to **Inger Erickson** and said those are great issues and questions, however if you are uncertain about office, do not title it “office”. Our mind(s) should shift to want residential in the area. **Inger Erickson** asked why not residential in the area. **Alex Leeman** said financial and tax base is one item, commercial will help balance that for the City. The other item is the road infrastructure; no City has the type of road Commerce Drive will be, in their residential areas. **Alex Leeman** states that he remembers a few years ago with UDOT, saying they need to put another freeway in; people were upset about it. Having an office park here will allow more people to work in the area and then decrease those on the freeway. We have want a community where they are not solely half-acre homes; there is a need to have a diverse and sustainable community where businesses can succeed also. **Dave Petersen** stated that our population is projected going to double in the future and we are working towards building for our future citizens.

**Roger Child opened the Public Hearing at 8:29 PM.**

**(Zoom) Kyle Stowell** (1764 W Burke Lane) said that in 2011 when this area was rezoned that they would do tiered approach to building heights to transition from single-family homes, to medium density, to high density. He also

mentioned that with previous earthquakes, the land has shifted. He asked if there could be a limit on building heights and questioned if, we should put this on hold until we hear the PMP West agenda item.

**(Zoom) Lori Conover** (469 Quail Run Road) asked if the residents and/or City officials would have a say in the final look of the buildings, since Farmington has a different “feel” to the community rather than the developer’s previous projects in Lehi. She stated that she is confident that most of Farmington does not want black glass buildings, rather some office buildings with Farmington Rock.

**(Zoom) Ben Rollins** (908 Lands’ End) lived in Lehi he stated, and is residing in Farmington now. He said we are missing a “Downtown Davis” community and missing a gravity to pull companies into the town to give those residents the option avoid commuting. He said in Lehi, he did not like driving around so much to go get lunch for example, he asked if this would be envisioned as a downtown Davis community and how can this project avoid multiple parking lots and have a more walk-able community?

**(Zoom and Email) Heidi Herron** (926 N 1875 W) asked what the minimal building height is, and does not want it to restrict on the residential. She had also emailed some concerns that will be included in the record.

**Roger Child closed the Public Hearing at 8:40 PM.**

**Dave Petersen** answered the questions as a whole. Why we are not doing the east and west as one is because this is a lot of land. In the PMP East, there are 93.40 acres, and in the PMP West, there is 49.4 acres that we are considering, the next PMP (west) agenda item will happen, on our next meeting. The developer has entitlements to do six stories in the OMU zone, as we looked along I-15, that the railroad abutment is so high that the appearance over the top of the freeway would look like 2 to 3 story buildings. The City will look at the buildings for final approval and we will review the building designs with SPARC (Site Plan and Architect Review Committee). The hope is to get a Farmington look, something that will be timeless to the City. **Dave Petersen** also replied to the Farmington slide earthquake question, he said it starts at Smiths and stretches to most of the Farmington Ranches, the Bay, and to 400 W in the east. It comes close to 2/3 of the community.

**Andrew Bybee** said he wanted to provide a balance of what the community wants to see and also what tenants want to lease; they want natural light and large windows because it will increase productivity and it is refreshing. He said that **Ben Rollins** nailed the “Downtown Davis County” concept; they want to apply a SMART development here in Farmington. Looking at development needs to be multiple components, not just retail, office or residential. Their goal is to have people love the area and want to be here. **Andrew Bybee** said the idea here is not to create a Lehi parking lot feel (drive-in and drive-out); it has to provide a walk-able community that includes scooters and bikes.

**Alex Leeman** said he has two concerns at this time; his first concern is that he expected to see this, as a whole instead of East and West. The long-standing concern was the transitioning from homes to office; he asked if there is only one residential developer on this project? He also asked how we could tie these together. Can we condition the approval on the approval of the PMP West? **Dave Petersen** said that is a great thought and that we could see the PMP West beforehand. **Brigham Mellor** spoke on the advantage for their request on residential, he said if the developer wants any residential, we still have the ability to leverage their request for any criteria that is requested. The City has the ability to do a trade-off. Any residential right now is non-existent. **Alex Leeman** said he has hardly any concern on the office-uses and the buildings near the freeway, but it is important to him, to have the western frontage protected. He wants to ensure that we have adequate trades to have the appropriate heights.

**Andrew Bybee** notes that on the West portion of the development, the height along the DRG&W is limited to 36 feet in height that would be the buffer zone. There will be an existing 50 feet no build zone with the natural gas easement, **Doug Thimm** (architect) said, an additional 200 feet after the Rio Grande Trail. **Alex Leeman** said his second concern is he would like to see these two intertwined and under section 140 of our Development Agreements, it is supposed to

include “*proposed uses and intensity of uses*”. In this case, the terminology of uses that are allowed, are not included in the PMP. **Dave Petersen** said we could consult with **Todd Godfrey**, City Attorney, on that paragraph that **Alex Leeman** is questioning. Lastly, **Greg Wall** asked **Dave Petersen** if there are plans for restaurants, and **Dave Petersen** replied yes, there is possibility for 135,000+ square feet of retail use such as restaurants, drive thru options and a possible grocer. **Roger Child** said that he thinks it is important to keep things moving because we cannot ignore the fact that this area will be developed. He said he was asked to participate in a survey over 20 years ago, that asked how they want West Farmington to look, and he applauds the City for always looking towards the future.

**MOTION**

**Project Master Plan/Development Agreement**

3. *STACK Real Estate – Applicant is requesting a recommendation for approval of the North Farmington Station East Project Master Plan (PMP), and accompanying development agreement, a mixed-use development encompassing approximately 104 acres next to the west side of I-15, north of Burke Lane, east of the general vicinity of 1525 West Street, and north towards the Shepard Lane area. (PMP-2-20)*

**Alex Leeman** made a motion to move that the Planning Commission recommend that the City Council approve the enclosed PMP, and accompanying development agreement subject to all applicable Farmington City development standards and ordinances and that the applicant shall incorporate any comments from the City’s Development Review Committee (DRC), Site Plan and Architectural Review Committee (SPARC), and the City Attorney. Subject to the condition that this approval is conditioned upon a subsequent PMP for the North Farmington Station West Project Master Plan.

**Larry Steinhorst** seconded the motion, which was unanimously approved.

**Zoning Map Amendment**

4. *STACK Real Estate – Applicant is requesting a recommendation for Zoning Map Amendment approval of approximately 36.5 Acres of property in the vicinity of Spring Creek between 1525 West and I-15 from A (Agriculture) to OMU (Office Mixed Use) and OS (Open Space). (Z-2-20)*

**Alex Leeman** made a motion to move that the Planning Commission recommend that the City Council approve the zone change for the 36.5 acres of property as requested with the following condition: The applicant must stake the proposed location of the trail(s) adjacent to Spring Creek from the UP tracks to the D&RGW Trail and thereafter upon a favorable site visit and inspection by staff, the City Council shall rezone an acceptable amount of property abutting the center line of Spring Creek to OS (Open Space).

**Larry Steinhorst** seconded the motion, which was unanimously approved.

**Findings for Approval:**

1. The developer desires to leverage proximity to I-15 by proposing office buildings visible from the freeway. This may also significantly increase the viability of the office park thereby enhancing the community’s likelihood of providing a daytime population for its retail areas and at the same time, shoring up Farmington’s property tax base creating a more stable and diversified local economy for the future.

*Moreover, the project will provide more employment opportunities here in Davis County, which may result in less congested modes of transportation and cleaner air for its residents.*

2. The Union Pacific and Frontrunner embankments significantly block the ability to see the project area by the freeway passerby on I-15. The developer is asking that the City allow the possibility of taller buildings next to the interstate, and the City concurs that such buildings should be visible from the freeway and substantial enough to accommodate a good employment population.
3. Transit is a key element to ensure the mixed-use office park's success. The North Farmington Station concept mirrors similar and existing successful projects across the country by providing a "front door/fixed transit stop" for its employees working and living in the area. The recommended PMP contemplates a one-stop shuttle directly linking the Front Runner station to a remote transit hub in the heart of the proposed mixed-use development.
4. To implement the vision in Findings 1, 2, and 3 above, the location of the principal five lane north to south street ("Commerce Drive") illustrated in the UDA plan, which street provides the necessary connectivity between the Park Lane Interchange area and the future Shepard Lane Interchange to ensure that the Park Lane interchange does not fail, must move further to the west to allow space for said office buildings. The shift causes a realignment, or ripple effect, to all streets in the area and provides causation for the City to consider an amendment to the regulating plan consistent with the PMP. The alignment is also consistent with plans presented at an open house by the City on June 2, 2020.
5. The UDA plan recommends that the City locate Commerce Drive to the east or west of the mixed-use area (one side or the other), so as not to limit the walkability, human scale and vibrant, interactive, central magnet part of the mixed-use district. A western shift in Commerce Drive just enough to allow space for the office building next to the freeway places it too close to the center of the district compromising the mixed-use/pedestrian core. Therefore, the proposed PMP places Commerce Drive further to the west away from the middle.
6. The more successful office parks now nationwide provide a considerable/major residential component for their employees; furthermore, such workers list housing and commercial uses integrated with, or in close proximity to office uses as a significant reason to work for any given employer. The applicant's plan offers strong residential alternatives in the very core of their development within walking distance of work, transit, restaurant and recreation opportunities. [Note: STACK proposes to expand the Legacy Trail, a regional facility, north to the Haight Creek Trail, and a cross-project trail adjacent to Spring Creek (which east to west system includes a village green/gathering area) connecting the two north to south regional trails---the Legacy Trail and the existing D&RGW Trail.
7. The PMP/Development Agreement caps the amount of possible residential acreage within the project to ensure that residential uses will only mix with part of the site thereby not limiting the potential for office uses poised to occur in this prime real estate area between two freeway interchanges.
8. The proposed North Farmington Station East Project Master Plan and Development Agreement is consistent with the stated intent and purpose of the Farmington City General Plan and Zoning Ordinance for this district: including a fine grained mix of uses such as office, retail, and residential, an emphasis on bringing activity to the street and enhancing walkability, placing parking to the rear of buildings, creating public spaces and nodes, enhancing open space and connectivity and providing a live/work/play environment, etc.
9. The proposed North Farmington Station East Project Master Plan balances residential and retail, supporting the primary office use, which is the overarching intent of the OMU zone.
10. The fine-grained mixture of uses proposed in the North Farmington Station East Project Master Plan creates an office park that is unique to the State of Utah and will create a vibrant employment base for Davis County that fosters a live/work/play environment.

11. The proposed North Farmington Station East Project Master Plan will help to diversify and balance the City's tax structure through expanding its commercial property tax base, instead of relying too heavily on residential property and commercial sales tax.
12. The proposed PMP, development agreement, and the zone change are 1) reasonably necessary, 2) in the public interest, and 3) consistent with the city general plan and in harmony with the objectives and purpose of the zoning ordinance.

**OTHER BUSINESS**

**Item #5 Miscellaneous, Correspondence, Etc.**

1. This was discussed during the study session.

**ADJOURNMENT**

**Larry Steinhorst** made a motion to adjourn at 9:14 PM. **Alex Leeman** seconded the motion, which was unanimously approved.

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Roger Child, Planning Commission Chair

Discussion item b. (1)

PROGRESS SET	PRINTED DATE 05.21.2020	2005 EAST 2700 SOUTH   SUITE 200 SALT LAKE CITY, UTAH 84109 P&O: 877.0795   WWW.LAYTONDAVIS.COM	1454 S 200 E Farmington, Utah	CHRONOLOGY	PROJECT NO 20,078	DWN BY/CHK BY JLD/JLD	TITLE SITE PLAN	24X36 SHEET # A001
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Discussion item b (2)





# item 3+4



Carly Rowe <crowe@farmington.utah.gov>

## PMP For Stack Real Estate

2 messages

heidi herron <hjherron@yahoo.com>

Thu, Jun 11, 2020 at 11:40 AM

Reply-To: heidi herron <hjherron@yahoo.com>

To: "rchild@farmington.utah.gov" <rchild@farmington.utah.gov>, "aleeman@farmington.utah.gov" <aleeman@farmington.utah.gov>, "rhome@farmington.utah.gov" <rhome@farmington.utah.gov>, "rworkman@farmington.utah.gov" <rworkman@farmington.utah.gov>, "mplaizier@farmington.utah.gov" <mplaizier@farmington.utah.gov>, "gwall@farmington.utah.gov" <gwall@farmington.utah.gov>, "lsteinhorst@farmington.utah.gov" <lsteinhorst@farmington.utah.gov>, "ierickson@farmington.utah.gov" <ierickson@farmington.utah.gov>, "crowe@farmington.utah.gov" <crowe@farmington.utah.gov>

Cc: David Petersen <dpetersen@farmington.utah.gov>

Dear Farmington Planning Commission:

I am hoping to attend the meeting tonight, but currently my schedule isn't allowing for it, so I wanted to send my comments and questions through e-mail in case I am not able to make it. Also, on your agenda it states it is an application and doesn't state it is a public hearing for agenda items 3&4, so I am not sure my comments would be heard anyway. (correct me if I am wrong on that)

On your agenda item #4, rezone of the Agricultural, while I, along with most residents would love to have open farms and fields, I am realistic enough to know that won't happen, and the agricultural zone they are discussing is boxed in by OMU and is the best place for the office buildings (closer to the freeway), so that rezone makes sense.

In regards to agenda item #3, the master plan. I am trying to determine what exactly is being asked for in this application. I understand they want approval of their PMP, but what exactly are you approving? They have OMU already, so are they asking for approval of the mixed/residential? If so, and you give it now, does that take away any discussion over heights, spacing, capacity, appearance...etc? Are we giving them residential and getting no concessions in return? Page 30 of their application appears as though it is addressing the Section 140 restrictions. Will approving this PMP application take away public comments and city approval for those Section 140 restrictions if this is approved?

With the Section 140 provisions it gives the city the ability to deviate from OMU and approve residential, but usually when that is done, it is with more specific definitions or a more specific proposal of what exactly is being built. On page 10 of their proposal it shows Mixed-Use Commercial/Residential as 3 stories minimum. If approving this application means that provision is approved then I am firmly opposed to this application. The class A offices being set at a minimum of 5 stories I feel is way too high and the Mixed-Use Commercial/Residential as 3 stories minimum is way too high (as minimums). Having a 5 story building by the freeway is livable, but requiring them to be at least that high is a lot. Same thing with the Mixed-Use Commercial/Residential as 3 stories minimum is really high and restricts the layout and appearance of these units. I don't see a need in the middle of this OMU area for a minimum height on any of the buildings. I understand the city wants taller buildings by the freeway, and the developer does too, so a minimum of 3 stories there is reasonable, but 5 stories is really high and might not be doable depending on the market. If 5 stories aren't a feasible option for the developer, the middle ground with 3 stories will become the more desirable place to build and we won't get our office buildings by the freeway anytime soon. The goal, as residents (I believe) is the same things we have been asking for since this was zoned OMU, a tiered approach with a buffer between single family homes and this business park.

In regards to that I don't see a problem with the overall map layout, it is the details that have me concerned (although I am curious what exactly hospitality consists of since the definition was broad ranging from hotels to storage units). I think overall the map/layout works towards the residents desires, but the details are where I feel it falls short.

Sorry that was somewhat long winded, essentially my objections to the specifics of the proposal are as follows:

Based on the Section 140 petition on page 30 of their proposal:

Residential Use as shown in the Mixed-Use Commercial/Residential area of the Land Use Plan : in general yes, but details on the actual development heights, density and appearance should be the ultimate determining factor/approver. Also we have enough large townhome buildings and there is nothing in this language (that I can see) that prevents another large townhome development being the only thing going in there and not any office buildings.

The Building Height requirements are to be as follows: • Interstate 15 Frontage/Class A Office – Minimum Height of 5 stories - Too tall as a minimum and you run the risk of it never getting developed or taking so long it undermines the intent of bringing them in. Without a market for these large office buildings there is nothing to prevent them building smaller ones in the other areas that have lower minimums. I would ask this be lowered to 3 stories.

• Mixed-Use Commercial/Residential – Minimum Height of 3 stories. - a lot of our developments, both existing and going in have a blend of 2 story and 3 story townhomes and they look good and fit the market/area. This eliminates the ability for that blend and leaves only 3 story townhomes/apartments. Density, and height should be discussed on a per project basis and no minimum for this area should be set. It isn't by the freeway and doesn't need to be visible to draw people in, so why have the minimum requirement at all? I would ask this be removed.

The Building Siting Requirements are to be as follows: • Interstate 15 Frontage will have NO REQUIREMENT for RBR • - I am not positive I understand what they mean by this. Are they talking billboards/signage? If so, we had a lot of discussion as a city/community regarding signage and billboards for Station Park, I feel the same discussions should be had in regards to this. A "No requirement" statement is REALLY broad and not something I feel should be included in this plan. Also, if it is signage is there anything that restricts signage without the buildings (will our view without the buildings being built be a row of billboards?). If I am misunderstanding this provision, I apologize.

The Open Space Requirements are to be as follows: • Interstate 15 Frontage will require 10% Open space, which includes the Legacy Trail/Bus shuttle lane width. - no issues with this.

Lastly, this proposal doesn't seem to address the land west of theirs, which is in between this proposal and the existing single family homes west of the DR&G trail. Does this proposal change that (which is currently just OMU I believe). Are there more defined plans for that area, or not at this time?

Thank you for your time.

Heidi Herron

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David Petersen <dpetersen@farmington.utah.gov>

Thu, Jun 11, 2020 at 1:52 PM

To: Meagan Booth <mbooth@farmington.utah.gov>, Shannon Hansell <shansell@farmington.utah.gov>, Carly Rowe <crowe@farmington.utah.gov>

FYI

**FARMINGTON CITY**  
**PLANNING COMMISSION**  
June 25, 2020  
**ELECTRONIC AND IN PERSON MEETING**

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**STUDY SESSION**

*Present: Chairman Roger Child, Vice Chairman Alex Leeman, Mike Plaizier, Larry Steinhorst, Greg Wall, Russ Workman, and Alternate Commissioner Inger Erickson. Staff: Community Development Director David Petersen, Recording Secretary Carly Rowe, Planning/GIS Specialist Shannon Hansell, Associate City Planner Meagan Booth, and Assistant City Manager/Economic Development Director Brigham Mellor. Rulon Homer was excused.*

**Miscellaneous Item: North Cottonwood Creek PUD (S-27-18)**

Craig North of North Cottonwood Creek PUD subdivision, along with the builder for the property discussed building elevations and potential house plans with the Commission. This subdivision is going to be located at approximately 368 W State Street, just south of the Lagoon campground.

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**REGULAR SESSION**

*Present: Chairman Roger Child, Vice Chairman Alex Leeman, Mike Plaizier, Larry Steinhorst, Greg Wall, Russ Workman, and Alternate Commissioner Inger Erickson. Staff: Community Development Director David Petersen, Recording Secretary Carly Rowe, Planning/GIS Specialist Shannon Hansell, Associate City Planner Meagan Booth, and Assistant City Manager/Economic Development Director Brigham Mellor. Rulon Homer was excused.*

Chairperson Roger Child opened the meeting at 7:00 PM.

**Item #1 Minutes**

Alex Leeman made a motion to approve the minutes from May 21, 2020. Mike Plaizier seconded the motion, which was unanimously approved.

**Item #2 City Council Report**

The City Council approved the Phelps-Van Otten Plat Amendment as well as the Farmington Station Parkway preliminary plat and exception. The Project Master Plan (East) for North Farmington Station, containing 100+ acres was tabled until the next meeting, in order to hear both East and West as one collective application. Lastly, the City will undergo a property trade with the property owners of a home located on 1525 W, where Commerce Drive will go in.

**SUBDIVISION**

**Item #3 Steve and Tammy Thomas (Public Hearing) – Applicant is requesting approval of a metes and bounds subdivision (lot split) of 0.55 acres of property located at 41 West State Street in the R-4 (Multiple Family Residential) zone.**

The property owner desires to subdivide his property at 41 West State Street. The property is .55 acres (23,958 square feet). The minimum lot size in the R-4 zone is 10,000 square feet for each single-family or two-family dwelling, plus 4,000 square feet for each additional dwelling unit to a maximum of 4 dwelling units per building per lot.

The applicant is requesting approval to build a second single family home to the south of the existing single family home and create a separate lot for each dwelling. However in doing so, the proposed lot for the existing single-family home (Parcel A) is 8,593 square feet or 1,407 square feet less than the minimum lot size of 10,000 square feet. Therefore, the applicant is seeking a seeking a special exception of this fixed dimension as per section 11-3-045 of the Zoning Ordinance.

Moreover, the southern lot (or Parcel B) will be a land locked parcel with no actual street frontage in violation the city ordinances unless the Planning Commission determines otherwise as per section 12-7-050 B which states in part, "Private streets shall not be permitted unless the Planning Commission finds that the most logical development of the land requires that lots be created which are served by a private street or other means of access, and makes such findings in writing with the reasons stated therein." Staff is recommending that the commission require an easement through Lot A in order to access Lot B to establish a buildable lot.

**Steve Thomas**, the applicant (65 W State St) had no further explanation on the background of the item but thanked the City for their efforts. **Roger Child** asked what the width would be of the access for Lot B. **Steve Thomas** replied that it is currently 16 feet and he is working with UDOT to extend the driveway to the road and widen to the fence line. **Roger Child** then asked what type of parking would be provided. **Steve Thomas** answered that they are working to put a car port in behind the current home so the occupant would essentially drive in and turn right to park in the cart port. **Dave Petersen** did include that the fire department will require at least 20 feet width for the driveway. **Greg Wall** questioned the existing shed; **Steve Thomas** stated that when the carport is built, the shed would be taken down. **Greg Wall** also addressed the public comment that was emailed to staff regarding a maintenance agreement, indicating that it was a wise idea.

**Roger Child opened the public hearing at 7:27 PM.**

**(Email) Ali Avery (41 W State St)** emailed comments that will be included at the end of the record.

**Roger Child closed the public hearing at 7:27 PM.**

#### **MOTION**

**Greg Wall** made a motion to move that the Planning Commission approve the proposed lot split at 41 West State Street and a special exception for the smaller lot size for Parcel A, subject to all applicable Farmington City development standards and ordinances, and the following conditions:

1. The property owner will dedicate an access easement acceptable to the city for access to Lot B through Lot A as well as obtain UDOT approval.
2. The property owner will provide a survey from a licensed surveyor demonstrating all easements necessary for utilities, fire department access, including turnaround access if required, etc.
3. The applicant shall submit a site plan providing all utility locations and appropriate storm drainage facilities for the new lot.
4. The applicant must address all DRC Comments.
5. The Findlay parcel (Davis County Tax ID #07-028-0075) adjacent to the west boundary line of the lot split is 8,712 square feet in size, less than the minimum lot size for the R-4 zone. The applicant's subdivision plans shows a boundary adjustment with this property. Any such boundary adjustment shall not result in a land area less than the existing parcel size for the Findlay lot.
6. *Enter into a maintenance agreement and record the agreement against the parcel(s) that will be lot A and B; regarding the access and utility easement that will pass over from A to B in the event that the property is sold.*

**Larry Steinhorst** seconded the motion, which was unanimously approved.

#### **Findings for Approval:**

1. The property is zoned R-4 and the parcel is being divided for the purposes of building a single family home. This will match the character of the surrounding residences. Furthermore, the historic home built in 1898 on the property will be preserved.
2. The lot split is in accordance with the Farmington City General Land Use Plan and Zoning Ordinances.

3. No undevelopable remnant parcels will remain in the lot split.
4. The existing lot widths of both parcels do not conform to the lot width standards in the R-4 zone. However, the proposed subdivision will not increase the extent of the non-conforming widths

## **OTHER BUSINESS**

*Alex Leeman made a motion to hear Item #5a prior to Item #4. Greg Wall seconded the motion, which was unanimously approved.*

**Item #5a Tim and Jenny Pace (Public Hearing) – Applicant is requesting a special exception to exceed the minimum driveway width for a proposed circular driveway at their home, located at 141 S Bonanza Rd in an AE (Agriculture Estates) zone. (M-4-20)**

The applicant requests two curb cuts for a circular driveway totaling 41 feet 4 inches (20 feet 8 inches each). The City's ordinance states, "On lots with at least the minimum width required in the zone, one additional driveway may be permitted providing that the sum of the width of both driveways does not exceed the maximum widths specified in subsection A1 of this section". The parcel width is 106 feet, which allows one additional driveway per the ordinance. However, the sum of both driveways exceeds 30 feet; therefore, the applicant requests a special exception, to increase the width by 10 feet.

A major concern with widening a driveway is pedestrian safety: the wider the driveway the longer distance a pedestrian has to traverse, creating a larger conflict area between an automobile backing out and a pedestrian. In this case, the two driveways are separated by 49 feet creating a significant refuge between the two driveways. Another significant concern is largely aesthetic. Single-family residential neighborhoods typically have roadway side treatments that include curb, gutter, sidewalk and park strip. If larger driveways become too pervasive, the character of the neighborhood often changes. In this case, there is ample park strip not only on this lot but in the surrounding neighborhood as well.

Regarding the Special Exception, the 11-3-045 states; "Purpose: A special exception is an activity or use incidental to or in addition to a principal use permitted in a zoning district; or an adjustment to a fixed dimension standard permitted as an exception to the requirements of this title; or a transfer of development right (TDR), or rights, established because of blight which results in an additional lot, or lots, or a dwelling unit, or units; or an adaptive reuse of a building or structure eligible, or that may be eligible, for the National Register of Historic Places so long as the adaptive reuse does not compromise such eligibility. A special exception has less potential impact than a conditional use but still requires careful review of such factors as location, design, configuration and/or impacts to determine the desirability of authorizing its establishment on any given site. This section sets forth procedures for considering and approving special exceptions to the provisions of this title."

**(Zoom) Tim and Jenny Pace**, the applicants (141 S Bonanza Rd) had no additional information to add to the background information that was given.

**Roger Child opened the public hearing at 7:35 PM.**

No comments received.

**Roger Child closed the public hearing at 7:35 PM.**

## **MOTION**

**Greg Wall** made a motion to move that the Planning Commission approve a special exception allowing an extension of an

existing driveway and associated curb cut up to an additional eleven (11) feet, subject to all applicable Farmington City ordinances and development standards and the following conditions:

1. The applicant shall obtain a Farmington City Excavation Permit prior to construction.
2. The circular driveway shall be no less than six feet from any side property line and demonstrate this requirement on the site plan.

**Mike Plaizier** seconded the motion, which was unanimously approved.

Findings for Approval:

1. The proposed driveway extension does not significantly increase safety issues.
2. There is a driveway adjacent to the proposed driveway however; there will be a significant refuge available for pedestrians between the driveways along this street.
3. Park strips are present in this neighborhood and the proposed extension would not significantly affect the current roadway side treatments.
4. The house currently sits on a dead end street allowing the circular driveway will be beneficial as a turn around.

**PROJECT MASTER PLAN APPLICATION**

**Item #4 STACK Real Estate (Public Hearing) – Applicant is requesting a recommendation for approval for the North Farmington Station West Project Master Plan (PMP), and accompanying development agreement, a mixed-use development, encompassing approximately 35 acres west of 1525 West Street, south of Haight Creek, and east of D.R.G.&W. trail (PMP-2-20).**

*Commissioner Russ Workman abstains from voting on this agenda item from STACK Real Estate due to personal interest.*

In November of 2016, Chartwell Capital and the City contracted with Urban Design Associates (UDA) to conduct a planning charrette, which produced a conceptual master plan for the 220+ acres of property north of Shepard Creek, west of the UP tracks, east of the D&RGW trail, and south of Shepard Lane. The charrette process involved receiving input from a number of stakeholders, including 13 property owners within and adjacent to the project area, city staff, local elected officials and representatives from Chartwell Capital. The result was a master plan document, or sub-area master plan to the City's General Plan, intended to guide and inform the development of a future mixed-use office park.

The above referenced applicant, STACK Real Estate, is now proposing a more specific Project Master Plan (PMP) encompassing some 35 acres of the UDA master plan area for the reasons set forth in the findings below.

On Tuesday, June 2, 2020, Farmington City held an open house to receive citizen input regarding a proposed realignment to "Commerce Drive", which is the major north to South Street, intended to connect 950 North to Park Lane. The UDA plan shows the corridor for this principle street close to I-15 and the U.P. tracks. The proposed alignment is located further west at 1525 West. The North Farmington Station West PMP places the Commerce drive corridor in an alignment consistent with the information presented at the open house.

As referenced above, the property is zoned OMU, which zone does not allow residential uses. However, as per Section 11-18-140 of the Zoning Ordinance residential uses are possible in this zone.

An issue remains regarding the development agreement---that is, the proposed office to residential ratio set forth in paragraph 5.b. of the enclosed development agreement. Although the applicant is in favor of such a ratio, he maintains that due to the present uncertain office market he cannot fulfill this commitment right now. This topic will be discussed in further detail at the meeting.

**Dave Petersen** explained how building heights are measured in Farmington. On site plan reviews, the City will measure to the midpoint, which is 27 feet maximum for a single-family home on a flat lot; therefore, some single-family homes could be upwards to 30 to 40 feet at the pitch. When the applicant proposes 36 feet office buildings in the buffer area, it will not be significantly higher than some single family homes. He went on to say that, we will talk about the item tonight but we can continue the discussion next meeting on July 9, 2020 and combine the two PMP and Development Agreement(s).

**Nathan Ricks** (2801 N Thanksgiving Way #100, Lehi UT) said that they are prepared to combine both the West and East items to present to us at our next meeting. As for the comments received regarding building heights, he reiterates **Dave Petersen's** comments and then explains that there is 90 feet between the back of fence to the residential area(s) that will also act as a buffer. The other comment was regarding 36-foot townhomes extended to the Shepard interchange, and that would act as a better neighbor to the existing residential, STACK does prefer to keep some commercial there at the interchange. **Inger Erickson** asked about the architectural style, **Nathan Ricks** replied that they are working solely on land use right now but will be providing site plan approvals when it is time for residential. **Alex Leeman** asked about the strip near 950 north, and raised concern of how small that area is; he said a suggestion is possibly a backyard of a townhome versus wedging in a small commercial building. **Brigham Mellor** said the City is still working with UDOT on the right of way on this strip of land, so the area in discussion could change.

**Alex Leeman** went on to discuss concerns in paragraph 5b in the development agreement, this paragraph requires construction of the office buildings at the same time as residential. STACK Real Estate does have some concerns at this time about the office building, due to the current state of working-from-home to help relieve COVID-19. He then stated that he has been a part of this process for 6+ years, and the City has been looking to have a sustainable economic and tax base for the next 75+ years. One compromise he has brainstormed is perhaps allowing some residential prior to the office – and to develop the “yellow” area as residential since it is not as impactful and that would allow for a transition from the neighborhoods.

Beyond that, **Alex Leeman** said he likes the overall plan, he looks at them together and cannot wait to see the offices near the freeway and the “town square” area. Once the details are finalized, it will be a great addition to the City. **Nathan Ricks** replied that STACK is a long-game developer, they take on large products and traditionally they are office developers and they would like to have the flexibility to allow the market to tell them what to do first, office or residential. He said they are committed and setting aside 29 acres along the freeway for office use. **Alex Leeman** said he is concerned because if office is not there first (or soon after), it will be difficult to try explaining to residents that this is an office park versus a multi-family complex area. The greatest fear for those who sit on City Council, he said is that in 10+ years someone saying they do not want to do office anymore.

**Roger Child** opened the public hearing at 8:28 PM.

**(Email) Heidi Herron** (926 N 1875 W) emailed her comments and Dave Petersen read them in as part of the record and they will be included at the end of the record. Comments were addressed in discussion between commissioners and the developer.

**(Zoom) Kyle Stowell** (1764 W Burke Ln) said emotionally, he wants to oppose this project and there is still an element of disbelief of having a large-scale project near his home. However, he feels that he is here to ask on behalf of other residents to minimize density in the area. Like last meeting, he mentioned when the area was rezoned in 2011, that there would be a buffer, transition, and tiered approach from building heights. He asked for clarification on the distance from the D&RG trail fence to the east side of the rail trail. There is a gas line easement included in the distance. He thanked the developer for limiting the building heights to 36 feet or less along the western portion. His concern about the mixed residential area will be 4+ stories, and he opposes 4+ story buildings west of 1525 W. He also wanted to echo **Heidi Herron's** comments about Farmington losing a small-town feel. He ended with saying that he understands that

progress happens but he wants to minimize impact on those who live close by and wanting to keep his small-scale farm. In addition, he asks if the developer can keep the dark sky initiative, and look into minimizing the lighting, so it does not affect the residents.

**City Councilmember, Shawn Beus** (1727 W Clark Ln) stated that he wanted to attend tonight to hear the Commission's comments in comparison with the comments from City Council. He went on to explain by City Council tabled the item; essentially the City Council found some defects in the development agreement that did not meet statues and it was not executable and there was no other choice then tabling. He said all are on the same page concerning the developer's idea to minimize impact. The issue is to take into consideration some ways to buffer the area, and appreciated **Alex Leeman's** comments about the OMU zone. The City Council also would like to hear both East and West together as one item at their next meeting.

**Roger Child closed the public hearing at 8:41 PM.**

**Dave Petersen** commented on the requirements in chapter 18; buildings that front the local road can be three stories high. Not all stories are the same, though, that is why the City measures heights and not per story.

**Trevor Evans** (2801 N Thanksgiving Way #100, Lehi, UT) answered that the distance from the back of the fence line to the east, side of the trail is 40 feet, and they have taken an additional 50 feet from the gas line (totaling 90). From there, an additional 200 feet would be in the buffer area.

**Greg Wall** asked the developer if they have considered indicating on the plat that residents west of the proposed area have horses and that it is a permitted use in that area. **Nathan Ricks** said they have not done that before but it would be something that they could look into to satisfy the parties in the area. **Greg Wall** also asked if the townhomes were surface parked or would have a garage. **Nathan Ricks** said that they have not gotten into specifics of a site plan yet but would hope to have garages since they would be on the higher-end of value. **Greg Wall** asked with the height requirement of 36-foot to the ridgeline, do they envision three story homes and what type of roofs, pitched or flat. **Nathan Ricks** replied yes to three-story and that economically he thinks flat roofs do have a modern feel, but he is not near the design phase at this time. **Greg Wall** asked if they intend to collaborate with someone to subdivide and sell any land for the residential component, **Nathan Ricks** replied that their intent is to keep the area and allow STACK to develop. **Greg Wall** asked if they have done any multi-family development since they specialize in office buildings. **Nathan Ricks** said that currently they are working on multi-family along with office but previously they have stuck solely with office buildings. **Greg Wall** asked what they hope the ratio to be (office to residential), **Nathan Ricks** replies that they hope to let the market decide what they can build. He understands there are some concerns with that, and have heard it from staff as well. He said office-building parking lots are only about 10% full at this point and not positive on who will continue to work-from-home; his hope is that offices will make a comeback, but it will look different due to COVID-19. **Nathan Ricks** said the area in Lehi that they purchased, they started in 2006 and got through the recession in 2008, and since then have built 14 office buildings, and this is something that they will get through as well. **Greg Wall** questioned how many office buildings the developer anticipates. **Nathan Ricks** replied with 4-8 office buildings. **Greg Wall** said that the City would likely want a ratio (office to residential) because it is more concrete, he also questioned if they had a timeframe for closing. **Nathan Ricks** said they would be hoping to close on all areas of land in September. Their agenda anticipates having approvals before the end of July via City Council.

**Roger Child** stated his opinion on the development agreement, he said there should be residential development rights when the developer has a certain amount of square footage of office and the quality of that square footage; assembling the land is a benefit to the City. The developer should have the right to develop some of that land as residential from day one because of how much the developer is investing and how it will benefit the City. Secondly, he said that having the residential mixed use (residential, services, etc.) in the same area could be beneficial when it comes to finding

tenants who want to pre-lease. If the environment is created, then we can capture tenants before they want to go elsewhere.

**Inger Erickson** said she is glad about the developer wanting to see which way the market goes and base which component comes first. She said she also agrees with **Roger Child** and should develop what they feel is right, first. In addition, her feeling is that the market is dictating residential right now and she likes the comments from the residents that were received. She said there had been some developments on the East side that have been concerning to residents and she said she did not feel like anyone listened. She does not want the residents to feel that way and believes the residents should have a say in what the developer designs. **Dave Petersen** clarified that the homes going in this area would likely be high-rent and asked her opinion on any moderate-income housing in the area. **Inger Erickson** said that with the population growth, it would be something to consider. She said as far as moderate-income housing, it could work, depending on having an HOA and maintenance. She said she previously lived in a moderate-income housing area that was great looking and it now looks awful due to no maintenance; she said putting restrictions would help with moderate-income housing so it can maintain a high level of class. She said it appears when homes are closer together they seem to be those that are not well cared for. **Alex Leeman** said that it depends, actually and there are homes in his area that are on a great deal of land that are not cared for; he said every area is different. **Inger Erickson** wanted to say Daybreak is an exception, saying moderate housing near the million-dollar homes; and that HOA there is a game-changer there. **Roger Child** said that it is more so financing instead of design when it comes to moderate housing and you can do tasteful moderate housing on a budget.

**Alex Leeman** said that after doing some research, about 23 lots in his neighborhood all taxed at 55% paid roughly \$69,000 in property taxes last year (together). Whereas Cube Storage, paid about the same amount at full tax base; the storage space does not have as much of an impact as the neighborhood when it comes to roads, etc. He also mentioned one parcel he looked at Station Park, belonging to Hyatt Hotel, Old Navy etc. sits on 6 acres. Alone, they paid \$608,000 in property taxes. If the City fills in this empty area with only residential, it will essentially raise taxes because the City would not be able to afford to do any improvements. Commercial tax-base in the City is a necessity, the area the developer is looking at is a blank slate and typically, single-family homes are not located against the freeway. One of these office buildings that is proposed will bring in an estimated \$1 to \$2 million in property tax alone, if we have 5 to 6 put in, the City will be prepared for any tax increase for the next 50+ years. There is an important reason why the City has envisioned this; the correct long-term plan for Farmington is an office park. **Nathan Ricks** replied that as the developer, they had been looking at this area for the last 5 years and they want to bring the SMART community to Farmington. He mentioned that some communities that have refused commercial are now losing money.

## **MOTION**

**Alex Leeman** made a motion to move that the Planning Commission continue the Public Hearing on July 9, 2020 where both East and West will be reviewed collectively.

**Greg Wall** seconded the motion, which was unanimously approved.

## **OTHER BUSINESS**

### **Item #5 Miscellaneous, correspondence, etc.**

#### **b. Other**

## **ADJOURNMENT**

**Alex Leeman** made a motion to adjourn at **9:41 PM**. **Inger Erickson** seconded the motion, which was unanimously approved.

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**Roger Child, Planning Commission Chair**



Item #4

Carly Rowe <crowe@farmington.utah.gov>

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## PMP For Stack Real Estate

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heidi herron <hjherron@yahoo.com>

Thu, Jun 25, 2020 at 1:42 PM

Reply-To: heidi herron <hjherron@yahoo.com>

To: "rchild@farmington.utah.gov" <rchild@farmington.utah.gov>, "aleeman@farmington.utah.gov" <aleeman@farmington.utah.gov>, "rhommer@farmington.utah.gov" <rhommer@farmington.utah.gov>, "rworkman@farmington.utah.gov" <rworkman@farmington.utah.gov>, "mplaizier@farmington.utah.gov" <mplaizier@farmington.utah.gov>, "gwall@farmington.utah.gov" <gwall@farmington.utah.gov>, "lsteinhorst@farmington.utah.gov" <lsteinhorst@farmington.utah.gov>, "ierickson@farmington.utah.gov" <ierickson@farmington.utah.gov>, "crowe@farmington.utah.gov" <crowe@farmington.utah.gov>

Cc: David Petersen <dpetersen@farmington.utah.gov>

I plan on being on the zoom call for tonight, but thought it easier to send my comments for the meeting regarding the West PMP through e-mail since my zoom had a few issues last time (on my phones end).

With the North Station Phase I Development agreement that was agreed to earlier this year the buildings against the rail trail and thus closest to the large single family homes on the west were modified from their original desire of three stories to two stories. The developer worked with neighborhood and the city to help create a tiered approach to these buildings where the two story units were near the homes and the larger connections of units and three stories were closer to the freeway.

In the city council notes it says "Doug Anderson noted that as discussed in the work session, because this is the first project in the area it is critical that it look good. He said it will set the tone for future surrounding development."

This is that future development. I believe that the people of Farmington should have the same transition from three story buildings to two stories as the homes by the last approved development did. I would GREATLY prefer the heights come down from 36'. However, the ultimate look and feel of transitioning from 5 story+ office buildings to 3 story+ office buildings and then 3 story townhomes does not then transition down to single family homes without looking and feeling off. The heights of the tallest single family home may hit that height, but it still looks like a single family home. A Three story townhome doesn't look or feel anything like a single family home. The compromise to me is instead of us getting single family homes behind us, is that they are allowed townhomes, but instead of them getting three stories they have a requirement that they can't be more than two stories against the rail trail. That is a HUGE area to not have anything under 3 stories and isn't a transition at all from large single family homes to the large business park with Apartments and Town Homes.

When this was originally rezoned I was told to be grateful, because with the original zoning I may have ended up with Apartments behind my house. It sounds like I still might but with tons of traffic, commercial area and other business related buildings as well. As a neighborhood in Farmington, we would like to blend with this new area and not have an us versus them feel.

I noticed on the city's comments it states "without the requirements of this PMP it allows for 3 story buildings without the height restrictions". That is true. It is also true that the developer is asking for residential in an area where it isn't approved. That is a HUGE giveaway to them. I believe we should get something in return. They want 3 story townhomes in a city that is getting inundated with them. They aren't compromising on what they want, the fact that they could do worse is not a good argument for giving away so much. PLEASE require they at least are required 2 story

homes(townhomes/condos..) on the far west side of this development, which abuts the trail, as we did the last developer.

Lastly, there is a strip of hospitality on the west side that runs all the way next to 950 North. I would ask that strip to the west not be allowed and have the yellow run all the way to 950 north. The vagueness of hospitality leaves it too open. To have a fast food restaurant or gas station right there next to the residential area is definitely not a transition, also this is right where the lanes start restricting back down to residential roads. It also would dramatically decrease the value of the homes abutting it. I understand it is a busy road, but the yellow that they want on that skinnier west strip can extend to 950 North without taking away from all of the other areas the city is allowing.

Thank you for your time.

Heidi Herron  
926 N 1875 W  
Farmington, UT 84025  
801-499-6826

**WORK SESSION:** A work session will be held at 6:00 p.m. in Conference Room #3, Second Floor, of the Farmington City Hall, 160 South Main Street. The public is welcome to attend. The agenda for the work session will be as follows:

1. Questions or concerns the City Council may have on agenda items.

## **FARMINGTON CITY COUNCIL MEETING NOTICE AND AGENDA**

Notice is hereby given that the City Council of **Farmington City** will hold a regular City Council meeting on **Tuesday, July 7, 2020, at 7:00 p.m.** The meeting will be held at the Farmington City Hall, 160 South Main Street, Farmington, Utah.

*Meetings of the City Council of Farmington City may be conducted via electronic means pursuant to Utah Code Ann. § 52-4-207, as amended. In such circumstances, contact will be established and maintained via electronic means and the meeting will be conducted pursuant to the Electronic Meetings Policy established by the City Council for electronic meetings.*

The agenda for the meeting shall be as follows:

### **CALL TO ORDER:**

7:00 Roll Call (Opening Comments/Invocation) Pledge of Allegiance

### **PRESENTATIONS:**

7:05 Final Tax Increment Disbursement to CenterCal from the Station Park RDA

### **SUMMARY ACTION:**

*(Items listed are considered routine in nature and will be voted on in mass unless pulled for separate discussion)*

7:15 Minute Motion Approving Summary Action List

1. Approval of Minutes from June 9, 2020
2. Federal Aid Agreement with UDOT for the 400 West and State Street Pedestrian Signal Crossing
3. Federal Aid Agreement with UDOT for the Park Lane Widening Project

### **GOVERNING BODY REPORTS:**

7:20 City Manager Report

7:25 Mayor Talbot & City Council Reports

**ADJOURN**

**CLOSED SESSION**

Minute motion adjourning to closed session for reasons permitted by law.

DATED this 3rd day of July, 2020.

**FARMINGTON CITY CORPORATION**

By:   
Holly Gadd, City Recorder

**\*PLEASE NOTE:** Times listed for each agenda item are estimates only and should not be construed to be binding on the City Council.

*In compliance with the Americans with Disabilities Act, individuals needing special accommodations (including auxiliary communicative aids and services) during this meeting, should notify Holly Gadd, City Recorder, 451-2383 x 205, at least 24 hours prior to the meeting.*

**Posted 07/03/2020**



## Planning Commission Staff Report July 9, 2020

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### Item 3: Sydney Corner Preliminary Plat

Public Hearing:	No
Application No.:	S-1-20
Property Address:	SW Corner of 650 West & Glover Lane
General Plan Designation:	RRD (Rural Residential Density)
Zoning Designation:	AE (Agriculture Estates)
Area:	.94 acres
Number of Lots:	4
Property Owner:	Fred and Jennifer Sanchez
Agent:	Guy Haskell – Updwell Development LLC

Request: *Preliminary Plat approval for the subdivision.*

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#### **Background Information**

The developer received schematic plan approval for the Sydney Corner Phase 2 Subdivision from the City Council on April 14, 2020. The Council followed the recommendation of the Planning Commission (see conditions 1-3 below and the status thereto in italics after each condition)

1. The applicant shall obtain approval of the TDR by the City Council;  
*Complete.*
2. Transfer lots considered as a special exception may only occur because of blight, and the applicant must complete a blight study, as defined and consistent with State Code, and the City must establish a finding of blight for the receiving area in order to approve such transfer. Also, the building inspector must inspect the property and provide the Council with a report, and that City staff explore a mechanism other than the TDR  
*Complete- A blight study was complete and the Planning Commission recommended the City Council approve a Text Amendment related thereto on May 21, 2020, but the Council has not yet considered this request.*
3. The applicant shall address all outstanding DRC Comments.  
*Incomplete-There are a few issues raised by the DRC which need to be addressed. Staff has included the items as conditions of the suggest motion.*

#### **Suggested Motion**

Move that the Planning Commission approve the Preliminary Plat for the Sydney Corner Phase 2 Subdivision subject to all applicable Farmington City development standards and ordinances and the following conditions:

1. The applicant must submit a final plat meeting all requirements set forth in Subdivision Ordinance.
2. The applicant must pay the amount, agreed upon by the City Council, for the TDR lot(s) to the City prior to or concurrent with the recordation of the final plat.
3. The applicant shall show the cross section and dedicate the corrected 650 West and Glovers Lane right-of-way on the final plat.
4. The applicant must abandon The US Bureau of Reclamation's (BOR) easement prior to final plat approval.
5. The applicant must submit a Soils Report with the final plat drawings.
6. The City's Development Review Committee (DRC) must approve final improvement drawings for the development, and the applicant shall complete any other DRC outstanding comments/requirements related to this, the development, and the final plat.

#### Findings for Approval

1. The submitted drawing is acting as the preliminary plat for the subdivision.
2. The parcel size is comparable to the existing Sydney's Corner Subdivision Phase 1.
3. Lot dimensions comply with the standards set forth in the Zoning and Subdivision ordinances because the City made a finding of blight.
4. The proposed average lot size for the project is .22 acres or 9,583.2 square feet, which required a special exception because each lot is less than the minimum alternative lot size of 12,000 square feet as set forth in the AE zone. The Planning Commission approved a special exception for the smaller lot size on March 5<sup>th</sup>.
5. All lots front an existing fully improved public right of way (650 West and Glover Lane).
6. The development mirrors the development across the street and is consistent with the General Plan, which development the City also approved 3 TDR lots to help clean-up blight.
7. The subdivision will not be detrimental to the health, safety or general welfare of persons residing or working in the vicinity, or injurious to property or improvements in the vicinity.

#### Supplemental Information

1. Vicinity Map
2. Preliminary Plat

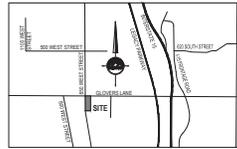
#### Applicable Ordinances

1. Title 11, Chapter 3 – Definitions
2. Title 11, Chapter 10 – Agriculture Zones
3. Title 11, Chapter 28 – Supplementary and Qualifying Regulations
4. Title 12, Chapter 7 – General Requirements for All Subdivisions
5. Title 12, Chapter 6– Major Subdivisions



# SYDNEYS CORNER SUBDIVISION PHASE 2

LOCATED IN THE SOUTHEAST QUARTER  
OF SECTION 25  
TOWNSHIP 3 NORTH RANGE 1 WEST  
SALT LAKE BASE & MERIDIAN  
FARMINGTON CITY, DAVIS COUNTY, UTAH



### SURVEYOR'S CERTIFICATE

I, **MICHAEL B. HERBST**, do hereby certify that I am a Licensed Land Surveyor, and that I hold certificate No. **56633** as prescribed under laws of the State of Utah. I further certify that by authority of the Owner, I have made a survey of the tract of land shown on the plat and described below, and have established said tract of land into lots, hereafter to be known as, **SYDNEYS CORNER SUBDIVISION PHASE 2**, and that the same has been correctly surveyed and staked on the ground as shown on this plat. I further certify that all lots meet front width and area re-requirements of the applicable zoning ordinances.

### BOUNDARY DESCRIPTION

A parcel of land, situate in the Southeast Quarter of Section 25, Township 3 North, Range 1 West, Salt Lake Base and Meridian, said parcel also located in Farmington, Utah, more particularly described as follows:  
Beginning at a point at the Southwest corner of the intersection of Glover Lane and 650 West Street, point being South 0°29'51" East 204.60 feet along the Quarter Section line and South 88°20'00" West 215.16 feet, said point being the Point of Beginning.  
Thence South 0°12'57" East 68.11 feet along the West line of 650 West Street.  
Thence South 88°20'00" West along the arc of a 364.72 foot radius non-tangent curve to the left (curve bears South 88°43'00" East and the long chord bears South 1°40'27" East 261.55 feet through a central angle of 3°54'22" along said West line.  
Thence South 89°20'00" West 157.50 feet.  
Thence North 0°29'51" West 330.00 feet to the South line of Glover Lane.  
Thence North 89°20'00" East 123.98 feet along the South line of said Lane to the Point of Beginning.

Contains 45,461 square feet or 0.93 acres, 4 Lots.

Date: \_\_\_\_\_  
Signed: J. Kinnaman  
License No. 334575

### OWNER'S DEDICATION

Known all men by these presents that I, the under- signed owner (s) of the above described tract of land, having caused same to be subdivided, hereafter known as:

## SYDNEYS CORNER SUBDIVISION PHASE 2

do hereby dedicate for perpetual use all easements shown here on Public Utility Easements on this plat as intended for Public use, in witness whereof I have hereunto set our hand (s) this \_\_\_\_\_ day of \_\_\_\_\_, A.D. 20\_\_\_\_.

By: **Upwell Development LLC**  
Guy M. Heskell, Managing Member

### LIMITED LIABILITY COMPANY ACKNOWLEDGMENT

STATE OF UTAH, J.S.,  
County of DAVIS  
On the \_\_\_\_\_ day of \_\_\_\_\_, A.D. 20\_\_\_\_, I, \_\_\_\_\_, personally appeared before me, the undersigned Notary Public, in and for said County of \_\_\_\_\_, in the State of Utah, who after being duly sworn, acknowledged to me that he/she is the \_\_\_\_\_ of \_\_\_\_\_, a Limited Liability Company and that he/she signed the Owner's Dedication freely and voluntarily for and in behalf of said Limited Liability Company for the purposes therein mentioned and acknowledged to me that said Limited Liability Company executed the same.

MY COMMISION EXPIRES: \_\_\_\_\_ RESIDING IN \_\_\_\_\_ COUNTY.  
NOTARY PUBLIC:

## SYDNEYS CORNER SUBDIVISION PHASE 2

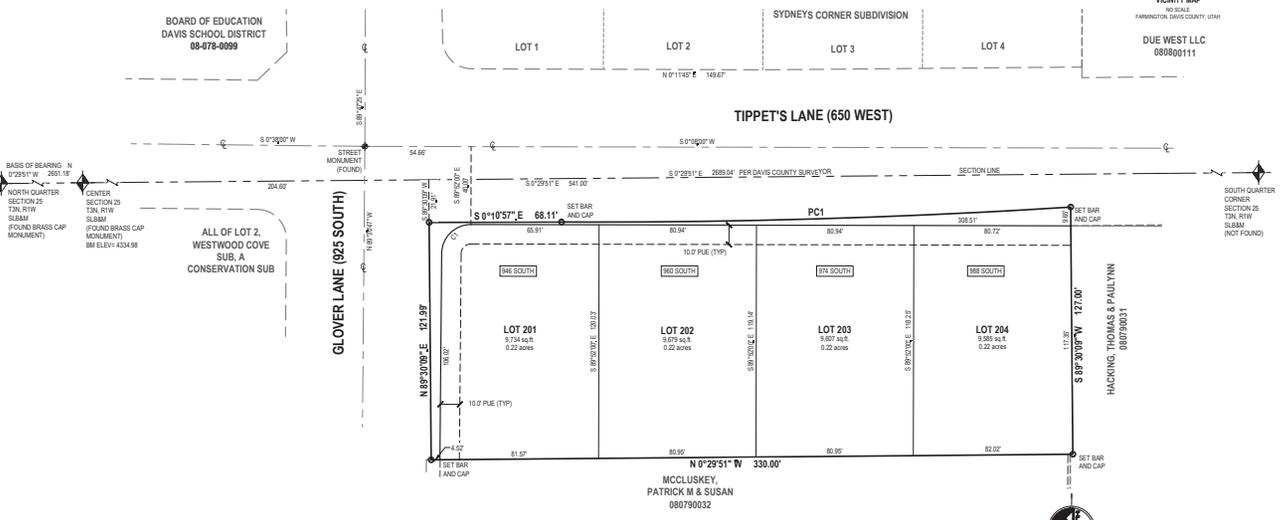
LOCATED IN THE SOUTHEAST QUARTER  
OF SECTION 25  
TOWNSHIP 3 NORTH RANGE 1 WEST  
SALT LAKE BASE & MERIDIAN  
DAVIS COUNTY, UTAH

### DAVIS COUNTY RECORDER

ENTRY NO. \_\_\_\_\_ FEE \_\_\_\_\_  
AND \_\_\_\_\_ FILED FOR RECORD AND  
RECORDED THIS \_\_\_\_\_ DAY OF 20\_\_\_\_  
AT \_\_\_\_\_ PAGE \_\_\_\_\_ OF OFFICIAL RECORDS

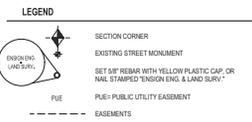
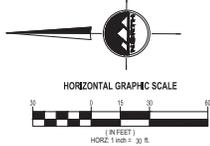
### DAVIS COUNTY RECORDER

BY \_\_\_\_\_ DEPUTY RECORDER



- #### GENERAL NOTES:
1. ALL PUBLIC UTILITY (PUE) ARE 10' FRONT UNLESS OTHERWISE NOTED HEREON.
  2. REBAR AND CAP WILL BE PLACED AT ALL LOT CORNERS.
  3. ALL SETBACKS SHALL MEET THE REQUIREMENTS OF CHAPTER 12 OF THE FARMINGTON CITY ZONING ORDINANCE.

NOTE: UTILITIES SHALL HAVE THE RIGHT TO INSTALL, MAINTAIN, AND OPERATE THEIR EQUIPMENT ABOVE AND BELOW GROUND AND ALL OTHER RELATED FACILITIES WITHIN THE PUBLIC UTILITY EASEMENTS IDENTIFIED ON THIS PLAT MAP AS MAY BE NECESSARY OR DESIRABLE IN PROVIDING UTILITY SERVICES WITHIN AND WITHOUT THE LOTS IDENTIFIED HEREIN, INCLUDING THE RIGHT OF ACCESS TO SUCH FACILITIES AND THE RIGHT TO REQUIRE REMOVAL OF ANY OBSTRUCTIONS INCLUDING STRUCTURES, TREES AND VEGETATION THAT MAY BE PLACED WITHIN THE P.U.E. THE UTILITY MAY REQUIRE THE LOT OWNER TO REMOVE ALL STRUCTURES WITHIN THE P.U.E. AT THE LOT OWNER'S EXPENSE, OR THE UTILITY MAY REMOVE SUCH STRUCTURES AT THE LOT OWNER'S EXPENSE. AT NO TIME MAY ANY PERMANENT STRUCTURES BE PLACED WITHIN THE P.U.E. OR ANY OTHER OBSTRUCTION WHICH INTERFERES WITH THE USE OF THE P.U.E. WITHOUT THE PRIOR WRITTEN APPROVAL OF THE UTILITIES WITH FACILITIES IN THE P.U.E.



**WEBER BASIN WATER DISTRICT NOTE:**  
SECONDARY WATER HAS BEEN ALLOCATED TO PARCELS BASED ON AN ANTICIPATED LANDSCAPE AREA (INCLUDING PARK STRIP) OF NO MORE THAN 60% OF THE GROSS PARCEL SIZE AND OF THE LANDSCAPE AREA IS PLANTED IN THE LANDSCAPE AREA (PLANTED IN LOW WATER USE PLANTS, LARGER TOTAL LANDSCAPE AREA, HIGHER PERCENTAGE TURF AREAS, INEFFICIENT DESIGN OF IRRIGATION SYSTEM, OR INEFFICIENT OPERATION OF IRRIGATION SYSTEM MAY RESULT IN HOMEOWNER INCURRING ADDITIONAL BILLING CHARGES AND/OR SECONDARY WATER SERVICE BEING SHUT OFF.

<b>WEBER BASIN WATER DISTRICT APPROVAL</b> APPROVED THIS _____ DAY OF _____, 20____ BY: THE WEBER BASIN WATER DISTRICT APPROVAL (SELECTION: WEBER BASIN WATER DISTRICT)	<b>CENTRAL DAVIS SEWER DISTRICT APPROVAL</b> APPROVED THIS _____ DAY OF _____, 20____ BY: THE CENTRAL DAVIS SEWER DISTRICT APPROVAL ENGINEER: CENTRAL DAVIS SEWER DISTRICT	<b>CITY ATTORNEY'S APPROVAL</b> APPROVED THIS _____ DAY OF _____, 20____ BY: THE FARMINGTON CITY ATTORNEY FARMINGTON CITY ATTORNEY	<b>PLANNING COMMISSION APPROVAL</b> APPROVED THIS _____ DAY OF _____, 20____ BY: THE CITY PLANNING COMMISSION APPROVAL CHAIRMAN: FARMINGTON CITY PLANNING COMMISSION	<b>CITY ENGINEER'S APPROVAL</b> APPROVED THIS _____ DAY OF _____, 20____ BY: THE FARMINGTON CITY ENGINEER FARMINGTON CITY ENGINEER	<b>CITY COUNCIL APPROVAL</b> APPROVED THIS _____ DAY OF _____, 20____ BY: THE FARMINGTON CITY COUNCIL CITY RECORDER CITY MAYOR	<b>SHEET 1 OF 1</b> PROJECT NUMBER: 0581 MANAGER: D. KINSMAN DRAWN BY: C. CHILD CHECKED BY: D. KINSMAN DATE: 2020-06-01	<b>DAVIS COUNTY RECORDER</b> ENTRY NO. _____ FEE _____ AND _____ FILED FOR RECORD AND RECORDED THIS _____ DAY OF 20____ AT _____ PAGE _____ OF OFFICIAL RECORDS DAVIS COUNTY RECORDER BY _____ DEPUTY RECORDER
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#### SURVEY RECORDING DATA

DATE: \_\_\_\_\_  
DRAWING NO. \_\_\_\_\_  
**TOOLE**  
 650 North Main Street Unit 1  
 Toole, Utah 84074  
 Phone: 435.643.2600  
 Fax: 435.578.0168  
 WWW.TOOLEINC.COM  
**SALT LAKE CITY**  
 Phone: 801.467.0100  
**LAYTON**  
 Phone: 801.467.0100  
**CELESTINE CITY**  
 Phone: 435.582.5500  
**PROVO CITY**  
 Phone: 435.582.5500





## Planning Commission Staff Report July 9, 2020

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### Item 4) --North Farmington Station Project Master Plan (PMP) and Development Agreement

Public Hearing:	Yes
Application No's.:	PMP-2-20
Property Address:	Area north of the vicinity of Burke Lane, west of I-15, east of the D.& R.G.W., and south of Haight Creek
General Plan Designation:	CA/BP (Class A Business Park)
Zoning Designation:	OMU (Office Mixed Use)
Area:	Approximately 142.79 Acres
Number of Lots:	n/a
Property Owner:	Multiple Property Owners
Applicants:	STACK Real Estate

Request: *Recommendation for approval of a project master plan and development agreement*

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#### **Background Information**

In November of 2016, Chartwell Capital and the City contracted with Urban Design Associates (UDA) to conduct a planning charrette which produced a conceptual master plan for the 220+ acres of property north of Shepard Creek, west of the UP tracks, east of the D&RGW trail, and south of Shepard Lane. The charrette process involved receiving input from a number of stakeholders, including 13 property owners within and adjacent to the project area, city staff, local elected officials, and representatives from Chartwell Capital. The end result was a master plan document, or sub-area master plan to the City's General Plan, intended to guide and inform the development of a future mixed-use office park.

On Tuesday, June 2, 2020, Farmington City held an open house to receive citizen input regarding a proposed realignment to "Commerce Drive", which is the major north to south street intended to connect 950 North to Park Lane. The UDA plan shows the corridor for this principle street close to I-15 and the U.P. tracks. The proposed alignment is located further west at 1525 West. The North Farmington Station West PMP places the Commerce drive corridor in an alignment consistent with the information presented at the open house.

As referenced above, the property is zoned OMU, which zone does not allow residential uses. However, as per Section 11-18-140 of the Zoning Ordinance residential uses may be possible in this zone.

Three Public Hearings. Previously the applicant divided the subject property into two PMPs---east and west. The Planning Commission held a public hearing and considered and recommended approval of the east PMP (104 acres), and accompanying development agreement, on June 11, 2020. At a second public hearing on June 23<sup>rd</sup>, the Commission considered the west PMP (35 acres), and accompanying development agreement, but continued action to allow time for the applicant to combine both as one PMP, and development agreement, for consideration at this the third public hearing.

A Major Issue. The timing of the initial office to residential ratio for the development, which was discussed at the previous two public hearings, remains a major issue. As referenced in earlier staff reports, the applicant is in favor of such a ratio, but maintains that due to the present uncertain office market he cannot begin the project with this commitment right now. The applicant’s proposed compromise is set forth in the enclosed draft development agreement and as follows:

- ~~○ Office to residential acreage ratio shall be regulated per the PMP – Exhibit B as follows: i) There shall be no Residential Use Structures constructed until the first Office Building has commenced construction in the Class A Office Land Use Area; at which point residential construction shall not exceed the ratio of 1 acre of Office to 3 acres of Residential Use; and ii) The ratio of Office Use to Residential Use shall not apply after the commencement of construction of a third office building.~~
- Developer shall be permitted to proceed with development plan review processes for the multi-family residential product, shown in orange in the PMP and located along Burke Lane, and, subject to maximum height limitation of 36 feet, in the location shown in yellow in the PMP and located along the Denver and Rio Grande right of way. The remainder of the residential development in the PMP will not be permitted until the first office building is constructed. Once the first office building is constructed, the remaining residential shall be permitted at a ratio of 3 acres of residential for every 1 acre of office. However, at no time shall the overall PMP residential to non-residential land use exceed 45% to 55%. The Developer shall submit plans for, and process to completion a building permit application for the first office building before December 31, 2023.
- All office buildings in the Property will be constructed with LEED, Energy Star and ASHTO standards in mind.
- The mixed-use commercial/residential remote transit hub area north of Spring Creek and fronting Digital Drive will be built as wrapped podium or other product that minimizes the area foot print needed for parking agreed to by the City Council.
- The Spring Creek Trail system shall be completed as the first abutting property develops.
- All architectural materials utilized shall be of a quality and standard fitting for a Class A Office and park and the following materials shall be specifically prohibited:

City Attorney Comments (added to the staff report on 7.8.20). The motion below is subject to conditions, which include, among others, that the applicant shall incorporate comments from the City Attorney. However, the City Attorney, Todd Godfrey, provided some comments to the development agreement prior to the meeting (July 7th), instead of after the meeting and the purpose of this added section to the staff report is to provide his input sooner rather than later [the development agreement has been updated to now reflect his comments (see redlines below)]. He restored references to the “office park” in a couple of locations and believes there is sensitivity on the issue of doing all we can to

assure the property develops in the intended way. He also added revisions to section 4.h. because he believes the City will want more comfort with the intended quality of the residential product and, to some extent, the entirety of the project. He has no problem with the change in approach that moves away from prohibited materials generally. But some commitment on the standard of the product will be necessary for the Planning Commission and the City Council, in his opinion. Staff recommends that the condition remain requiring City Attorney input because he may provide more comments based on the discussion by the Planning Commission at the meeting and the public hearing.

### **Suggested Motion**

Move that the Planning Commission recommend that the City Council approve the enclosed PMP, and accompanying development agreement subject to all applicable Farmington City development standards and ordinances and that the applicant shall 1) incorporate any comments from the City's Development Review Committee (DRC), Site Plan and Architectural Review Committee (SPARC), and the City Attorney; and 2) the City Council shall rezone an acceptable amount of property abutting the center line of Spring Creek to OS (Open Space).

### **Findings for approval (no particular order)**

1. The developer desires to leverage proximity to I-15 by proposing office buildings visible from the freeway. This may also significantly increase the viability of the office park thereby enhancing the community's likelihood of providing a daytime population for its retail areas and at the same time shoring up Farmington's property tax base creating a more stable and diversified local economy for the future.

Moreover, the project will provide more employment opportunities here in Davis County which may result in less congested modes of transportation and cleaner air for its residents.

2. The Union Pacific and Frontrunner embankments significantly block the ability to see the project area by the freeway passerby on I-15. The developer is asking that the City allow the possibility of taller buildings next to the interstate, and the City concurs that such buildings should be substantial enough to accommodate a good employment population.
3. Transit is a key element to ensure the mixed-use office park's success. The North Farmington Station concept mirrors similar and existing successful projects across the country by providing a "front door/fixed transit stop" for its employees working and living in the area. The recommended PMP contemplates a one stop shuttle directly linking the Front Runner station to a remote transit hub in the heart of the proposed mixed-use development.
4. 55 To provide for more intense development next to I-15 east of the PMP, the location of the principal five lane north to south street ("Commerce Drive") illustrated in the UDA plan, which street provides the necessary connectivity between the Park Lane Interchange area and the future Shepard Lane Interchange to ensure that the Park Lane interchange does not fail, must move further to the west to allow space for office buildings, etc. The shift causes a realignment, or ripple effect, to all streets in the area and provides causation for the City to consider an amendment to the regulating plan consistent with the PMP. The alignment is also consistent with plans presented at an open house by the City on June 2, 2020.

5. The UDA plan recommends that the City locate Commerce Drive to the east or west of the mixed-use area (one side or the other), so as not to limit the walkability, human scale and vibrant, interactive, central magnet part of the mixed-use district. A western shift in Commerce Drive just enough to allow space for the office building next to the freeway places it too close to the center of the district compromising the mixed-use/pedestrian core. Therefore, the proposed PMP places Commerce Drive further to the west away from the middle.
6. The mixed-use development begins with taller buildings next to I-15, then the height of the buildings steps down to lower density development near the D&RGW Trail---creating a lesser impact on single-family residential areas to the west.
7. The more successful office parks now nationwide provide a considerable/major residential component for their employees; furthermore, such workers list housing and commercial uses integrated with, or in close proximity to office uses as a significant reason to work for any given employer. The applicant's plan offers strong residential alternatives in the very core of their development within walking distance of work, transit, restaurant and recreation opportunities. [Note: STACK proposes to expand the Legacy Trail, a regional facility, north to the Haight Creek Trail, and a cross-project trail adjacent to Spring Creek (which east to west system includes a village green/gathering area) connecting the two north to south regional trails---the Legacy Trail and the existing D&RGW Trail.
8. The subject PMP proposes residential uses closest to the D.R.G.&W. trail as a transition area between less dense single-family residential neighborhoods to the west and more intense mixed-use development to the east of the PMP, and residential uses in this transition area shall not exceed 36 feet in height to the ridge line of the roof. As a point of reference, many single-family homes elsewhere in Farmington exceed this height, and the existing underlying zone without the requirements of this PMP allow for 3 story buildings which would likely surpass the 36-foot height threshold.
9. The PMP/Development Agreement caps the amount of possible residential acreage within the project to ensure that residential uses will only mix with part of the site thereby not limiting the potential for office uses poised to occur in this prime real estate area between two freeway interchanges.
10. The proposed North Farmington Station Project Master Plan and Development Agreement is consistent with the stated intent and purpose of the Farmington City General Plan and Zoning Ordinance for this district: including a fine grained mix of uses such as office, retail, and residential, an emphasis on bringing activity to the street and enhancing walkability, placing parking to the rear of buildings, creating public spaces and nodes, enhancing open space and connectivity and providing a live/work/play environment, etc.
11. The proposed North Farmington Station Project Master Plan balances residential and retail, supporting the primary office use, which is the overarching intent of the OMU zone.
12. The fine-grained mixture of uses proposed in the North Farmington Station Project Master Plan creates an office park that is unique to the State of Utah and will create a vibrant employment base for Davis County that fosters a live/work/play environment.

13. The proposed North Farmington Station Project Master Plan will help to diversify and balance the City's tax structure through expanding its commercial property tax base, instead of relying too heavily on residential property and commercial sales tax.
14. The proposed PMP and development agreement are 1) reasonably necessary, 2) in the public interest, and 3) consistent with the city general plan and in harmony with the objectives and purpose of the zoning ordinance.

**Supplemental Information**

1. Vicinity Map
2. UDA Master Plan
3. Existing Regulating Plan
4. North Farmington Station Development Agreement
5. North Farmington Station Project Master Plan (PMP)



**VICINITY MAP**  
**North Farmington Station PMP**



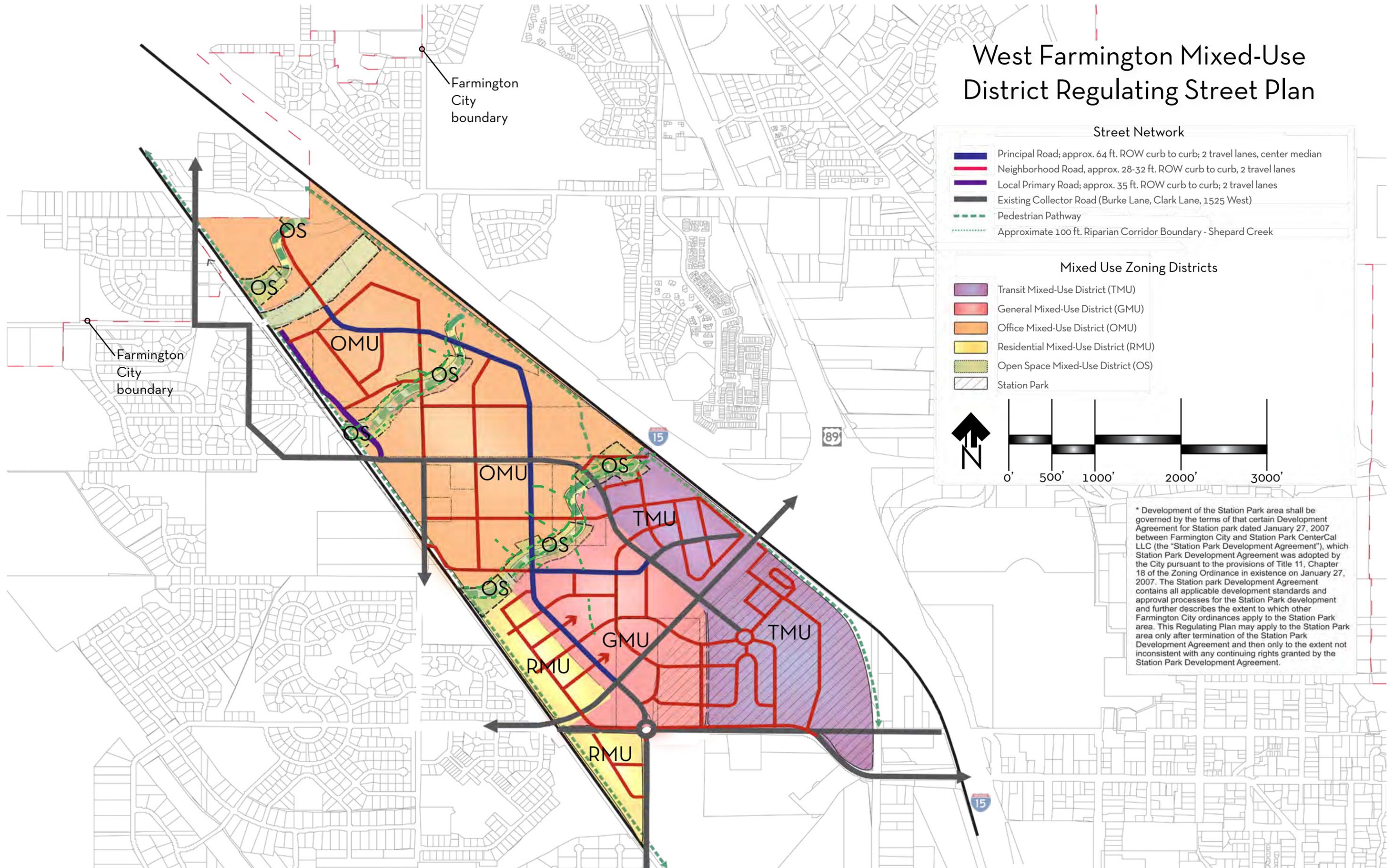
Disclaimer: This map was produced by Farmington City GIS and is for reference only. The information contained on this map is believed to be accurate and suitable for limited uses. Farmington City makes no warranty as to the accuracy of the information contained for any other purposes.



# ILLUSTRATIVE PLAN

1 DECEMBER 2016  
NORTH STATION MASTER PLAN | FARMINGTON, UTAH

# West Farmington Mixed-Use District Regulating Street Plan

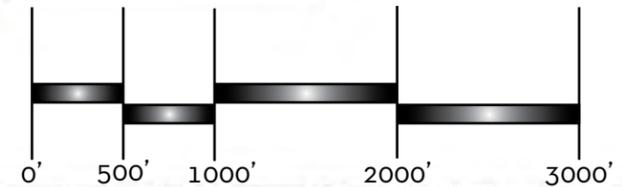


## Street Network

- █ Principal Road; approx. 64 ft. ROW curb to curb; 2 travel lanes, center median
- █ Neighborhood Road, approx. 28-32 ft. ROW curb to curb, 2 travel lanes
- █ Local Primary Road; approx. 35 ft. ROW curb to curb; 2 travel lanes
- █ Existing Collector Road (Burke Lane, Clark Lane, 1525 West)
- - - Pedestrian Pathway
- · - · - Approximate 100 ft. Riparian Corridor Boundary - Shepard Creek

## Mixed Use Zoning Districts

- █ Transit Mixed-Use District (TMU)
- █ General Mixed-Use District (GMU)
- █ Office Mixed-Use District (OMU)
- █ Residential Mixed-Use District (RMU)
- █ Open Space Mixed-Use District (OS)
- █ Station Park



\* Development of the Station Park area shall be governed by the terms of that certain Development Agreement for Station park dated January 27, 2007 between Farmington City and Station Park CenterCal LLC (the "Station Park Development Agreement"), which Station Park Development Agreement was adopted by the City pursuant to the provisions of Title 11, Chapter 18 of the Zoning Ordinance in existence on January 27, 2007. The Station park Development Agreement contains all applicable development standards and approval processes for the Station Park development and further describes the extent to which other Farmington City ordinances apply to the Station Park area. This Regulating Plan may apply to the Station Park area only after termination of the Station Park Development Agreement and then only to the extent not inconsistent with any continuing rights granted by the Station Park Development Agreement.

**DEVELOPMENT AGREEMENT**  
**FOR**  
**NORTH FARMINGTON STATION**

**THIS DEVELOPMENT AGREEMENT** (this “Agreement”) is made and entered into as of the \_\_\_\_ day of \_\_\_\_\_ 2020 by and between **FARMINGTON CITY**, a Utah municipal corporation, hereinafter referred to as the “City,” and **STACK REAL ESTATE, LLC**, a Utah limited liability company, hereinafter referred to, collectively with its assignees, as “Developer.”

**RECITALS:**

A. Developer has the right to acquire approximately 128 acres of land, and the City and others own the remaining land, within the boundary set forth in **Exhibit “A”** attached hereto and by this reference made a part hereof (the “Property”), Developer desires to develop the Property pursuant to the City’s Land Use Master Plan and the City’s Ordinances, as a Class A [office park and S.M.A.R.T](#) (Sustainable, Mixed Use, Attractive, Realistic, Transit-Oriented) community including office, multi-family apartments, and supporting retail and complementary uses, to be known as “North Farmington Station”.

B. The Developer has pursued two separate applications before the Planning Commission for the Property represented as North Farmington Station East and North Farmington Station West. The two PMP applications have been combined for consideration by the City Council in one application for approval of North Farmington Station.

C. On \_\_\_\_\_, 2020, the City approved a Project Master Plan (the “PMP”) for the Property in accordance with Chapter 18 of the City’s zoning ordinance. The approved PMP is attached hereto as **Exhibit “B”** and incorporated herein by reference. The purposes of the PMP includes, among other things, the establishment of uses and minimum building heights applicable to the respective areas of the Property, as set forth in the PMP, although the PMP is not intended to enable future development of the Property without final subdivision and site plan approval with respect to each phase.

D. The Property is subject to the City’s Laws, including without limitation Section 11-18-140 of the City’s zoning ordinance, pursuant to which this Agreement may control over certain provisions of the City’s Laws with respect to the matters set forth herein.

E. Persons and entities hereafter developing the Property or any portions of the Property shall accomplish such development in accordance with the City’s Laws and the provisions set forth in this Agreement.

F. The City also recognizes that the development of North Farmington Station, and any future phase thereof, may result in tangible benefits to the City through the stimulation of development in the area, including a possible increase of the City’s tax base and the development of amenities that may enhance further economic development efforts in the vicinity of the

Property, and is therefore willing to enter into this Agreement, subject to the terms and conditions set forth herein.

## AGREEMENT

**NOW, THEREFORE**, in consideration of the mutual covenants contained herein and other good and valuable consideration, the receipt and sufficiency of which are hereby acknowledged, the City and Developer hereby agree as follows:

1. **Incorporation of Recitals.** The above Recitals are hereby incorporated into this Agreement.

2. **Definitions.** In addition to the other capitalized terms defined elsewhere in this Agreement, the following terms shall have the respective meanings indicated below:

a. “City’s Laws” means, collectively, all City ordinances, rules and regulations, including the provisions of the City’s General Plan, the City’s zoning and subdivision ordinances, the City’s engineering development standards and specifications, and any permits issued by the City pursuant to the foregoing ordinances and regulations.

b. “Effective Date” has the meaning set forth in Section 3.

3. **Effectiveness.** This Agreement, including the PMP, shall become effective for the respective parcel on the date that Developer acquires fee title to all of the following parcels (as identified pursuant to a Davis County Assessor property search): Parcel ID 08-058-0020, 08-058-0016, 08-060-0026, 08-060-0003, 08-057-0015, 08-057-0053, 08-057-0046, 08-057-0064 (the “Effective Date”).

4. **Uses of the Property.** The uses of the Property and the respective areas of the Property designated for each such use shall be as set forth in the PMP. Specific Development Standards and Processing requirements shall be as follows:

a. **Building Height Limits.** Minimum building heights shall be regulated per the PMP – Exhibit B.

b. **Office to Residential Acreage Ratio.** Developer shall be permitted to proceed with development plan review processes and construction upon final approval for the multi-family residential product, shown in orange in the PMP and located along Burke Lane, and, subject to maximum height limitation of 36 feet, in the location shown in yellow in the PMP and located along the Denver and Rio Grande right of way. The remainder of the residential development in the PMP will not be permitted until the first office building is ~~constructed~~under construction. Once the first office building is ~~constructed~~under construction, the remaining residential shall be permitted at a ratio of 3 acres of residential for every 1 acre of office. The overall PMP residential to non-residential land use plan will not exceed 45% to 55%. The Developer shall submit plans for, and process to completion a building permit application for the first office building before December 31, 2023.

c. All office buildings in the Property will be constructed with LEED, Energy Star and ASHTO standards in mind but ~~may not be certified~~ such certification shall not be required.

d. The mixed use commercial/residential remote transit hub area north of Spring Creek and fronting Digital Drive will be built as wrapped or podium or other product that minimizes the area foot print needed for parking.

e. The Spring Creek Trail system shall be sequentially completed as the abutting properties develop unless otherwise required as part of the subdivision/site plan review process.

f. All public infrastructure shall be constructed in strict accordance with the City's Standards and Specifications for Public Improvements.

g. Common Area Management Plans for the Property shall be approved by the City at the time final site plans are approved for portions of the Property. Those plans shall include, at a minimum, provisions for the maintenance and upkeep of private streets and common areas, snow removal and location, use and maintenance of common and/or shared access points.

h. All amenities associated with the residential development and all architectural materials utilized throughout the project shall be of a quality and standard fitting for a Class A Office and a top quality mixed use development.

i. Zoning Ordinance Sections as of Effective Date. Throughout the term of this Agreement, the Property shall be regulated per Title 11, Chapter 18, specifically Section 11-18-050 and Section 11-18-060 attached hereto as **Exhibit "C"** of Farmington City's code, as such Chapter reads as of the Effective Date and is incorporated herein by this reference; provided that, without limiting Developer's right to submit petitions or applications under Section 11-18-140 of the City's zoning ordinance, the provisions of this Agreement and the PMP shall control in the case of any conflict with such Chapter.

j. The City agrees to process an amendment to the Regulating Plan for all principal streets as shown on the PMP. The Amendment of the Regulating Plan relating to non-principal roads will be considered by the City as part of the further development plan review process for site plans or subdivisions.

5. **Assignment.** Developer shall not assign this Agreement or any rights or interests herein without giving prior written notice to the City. Any future assignee shall consent in writing to be bound by the terms of this Agreement as a condition precedent to the assignment.

6. **Notices.** Any notices, requests and demands required or desired to be given hereunder shall be in writing and shall be served personally upon the party for whom intended, or if mailed, by certified mail, return receipt requested, postage prepaid, to such party at its address shown below:

To Developer: STACK Real Estate, LLC  
Attn: Andrew Bybee or Trevor Evans  
2801 North Thanksgiving Way, Ste. 100  
Lehi, Utah 84043

To the City: Farmington City  
Attn: City Manager  
160 South Main Street  
Farmington, Utah 84025-0160

7. **Entire Agreement.** This Agreement together with the Exhibits attached thereto and the documents referenced herein, and all regulatory approvals given by the City for the Property, contain the entire agreement of the parties and supersede any prior promises, representations, warranties or understandings between the parties with respect to the subject matter hereof which are not contained in this Agreement and the regulatory approvals for the Property, including any related conditions.

8. **Construction.** Words in any gender are deemed to include the other genders. The singular is deemed to include the plural and vice versa, as the context may require. The headings contained in this Agreement are intended for convenience only and are in no way to be used to construe or limit the text herein. Use of the word “including” shall mean “including but not limited to”, “including without limitation”, or words of similar import.

9. **Non-Liability of City Officials, Employees and Others.** No officer, representative, agent, or employee of the City shall be personally liable to Developer, or any successor-in-interest or assignee of Developer in the event of any default or breach by the City or for any amount which may become due Developer, or its successors or assigns, for any obligation arising under the terms of this Agreement, unless it is established that the officer, representative, agent or employee acted or failed to act due to fraud or malice.

10. **No Third-Party Rights.** The obligations of Developer set forth herein shall not create any rights in and/or obligations to any persons or parties other than the City. The parties hereto alone shall be entitled to enforce or waive any provisions of this Agreement.

11. **Recordation.** This Agreement shall be recorded by the City against the Property in the office of the Davis County Recorder, State of Utah.

12. **Relationship.** Nothing in this Agreement shall be construed to create any partnership, joint venture or fiduciary relationship between the parties hereto.

13. **Term.** This Agreement shall become effective upon the Effective Date and shall continue in full force and effect from such date until the date that is thirty (30) years after the City’s completion of construction of the arterial and principal roads shown in the PMP, unless terminated earlier pursuant to Section 14 below.

14. **Termination.** Notwithstanding the foregoing, if Developer has not commenced development activities on the Property within five (5) years after the principal roads are completed, the City may request Developer to provide the City with reasonable plans and assurances that Developer will develop the Property in accordance with this Agreement. In such event, Developer shall have 120 days after receiving such request from the City to provide the City with such information. If Developer fails to respond to such request within such time period, or responds within such time period with plans and assurances that are unacceptable to the City in the City's reasonable discretion, the City may terminate this Agreement by giving written notice to Developer within sixty (60) days following the termination of the 120-day response period described above.

15. **Severability.** If any portion of this Agreement is held to be unenforceable or invalid for any reason by a court of competent jurisdiction, the remaining provisions shall continue in full force and effect.

16. **Amendment.** This Agreement may be amended only in writing signed by the parties hereto. The parties acknowledge that Developer intends to acquire additional parcels of real property located adjacent to or near the Property, and the parties desire that Developer develop such additional parcels of Property pursuant to this Agreement to facilitate the consistency of the development of the Property and such additional parcels. Accordingly, and subject to any required Project Master Plan approvals which may be required, the parties agree to amend this Agreement to include within the scope and definition of the "Property" hereunder any additional parcels of real property acquired by Developer or its affiliate within area of the City bounded by Shepard Lane on the north/northwest, Interstate 15 on the northeast, Park Lane on the southeast, and the Denver and Rio Grande Western Rail Trail on the southwest.

**IN WITNESS WHEREOF**, the parties hereto have executed this Agreement by and through their respective, duly authorized representatives as of the day and year first hereinabove written.

**"CITY"**

**FARMINGTON CITY**

ATTEST:

\_\_\_\_\_  
City Recorder

By: \_\_\_\_\_  
Mayor

**"DEVELOPER"**

**STACK REAL ESTATE, LLC**

By: \_\_\_\_\_  
Andrew Bybee, Manager

**CITY ACKNOWLEDGMENT**

STATE OF UTAH            )  
  :ss.  
COUNTY OF DAVIS        )

On the \_\_\_\_\_ day of \_\_\_\_\_, 2020, personally appeared before me H. James Talbot, who being duly sworn, did say that he is the Mayor of **FARMINGTON CITY**, a municipal corporation of the State of Utah, and that the foregoing instrument was signed in behalf of the City by authority of its governing body and said H. James Talbot acknowledged to me that the City executed the same.

\_\_\_\_\_  
Notary Public

**DEVELOPER ACKNOWLEDGMENT**

STATE OF UTAH        )  
  :ss.  
COUNTY OF DAVIS    )

On the \_\_\_\_\_ day of \_\_\_\_\_, 2020, personally appeared before me Andrew Bybee, who being by me duly sworn did say that he is a manager of **STACK Real Estate, LLC**, and that the foregoing instrument was signed in behalf of said limited liability company by virtue of the authority granted to such manager under the operating agreement of said limited liability company, and he acknowledged to me that said limited liability company executed the same.

\_\_\_\_\_  
Notary Public

**ATTACHED EXHIBITS:**

- EXHIBIT “A” – LEGAL DESCRIPTION OF THE PROPERTY
- EXHIBIT “B” – PMP (PROJECT MASTER PLAN)
- EXHIBIT “C” – SECTION 11-18-050 AND SECTION 11-18-060 OF FARMINGTON CITY ZONING ORDINANCE

**STACK**  
REAL ESTATE



**P S O M A S**

# **NORTH FARMINGTON STATION**

## **Project Master Plan (PMP)**

JULY 1, 2020

July 1, 2020

**To the Farmington City Mayor, City Council, Planning Commission**

In Care of Mr. David Peterson, Community Development Director  
Farmington City Hall  
160 South Main Street  
Farmington City, Utah 84025

**Re: North Farmington Station  
STACK Real Estate Project Master Plan Submittal**

We are pleased to submit our Project Master Plan (PMP) for the North Farmington Station to Farmington City. Accompanying this, you will find our complete PMP Submission along with our Petitions for Rezoning and Alternative to the Approval Process (Section 140). We are excited to be partnering with Farmington City in taking the next steps forward in bringing the long-envisioned North Farmington Transit Oriented Development to fruition. We believe that this Project Master Plan is possible due to the foresight of the City to recognize the importance of this district and to bring forth the tools needed to bring it to pass in terms of City Planning and City Engineering and City Vision.

We have assembled a Development Team to work with Farmington City that is absolutely invested in the same long-term Vision:

STACK Real Estate has developed millions of square feet of Transit Oriented property all along the Wasatch Front including the Thanksgiving Point Lehi Transit Station District and the South Jordan Transit Station District, along with ongoing future developments all along the state's transit corridor.

Architectural Nexus has been involved with the Farmington City team in establishing the roots of what the district is growing into with their planning and design work at Station Park along with continuing planning and design work with TOD sites through-out the region.

We had the opportunity to visit Transit Oriented Development, along with Farmington City Officials, in Denver as a Public & Private Team to see some examples of what is happening along Denver's transit corridor, particularly at the Transit Stations. We were able to draw comparisons between what we had seen in Colorado and the many examples of Transit Oriented Development around the country. That said, we were also able to discuss the unique attributes of the North Farmington Station District and the applications that will make this a truly exceptional place. We are pleased that Farmington City has captured the vision and truly understands the importance of creating Transit Oriented Development that embraces the idea of creating an unquestionably spectacular place.

In order to do all of this, we have been genuinely fortunate to have been able to capture control of enough property to really do this right – that is over 130 acres. Our PMP brings with it the ability to completely ignore the “historic” property lines and to take the majority of the remaining area in the Mixed-Use District and to develop it in partnership with Farmington City into the fulfillment of an extraordinary vision. That is bringing Transit Oriented Development to the level that it should be. This PMP brings the intensity of a Class A Tech Office/Employment Center with one million square feet of office use along with a sustainably connected and walkable neighborhood residential development that is the proven next step in capturing the essence of what North Farmington Station should be. All of this, includes bringing the street and trail network completely thru the area and developing a permanent connection to the UTA station that is already established.

We would be remiss if we did not mention our appreciation to the many Members of the City's team who have worked with us to this point and look forward to a long and rewarding experience together. And, we are genuinely appreciative of you and your efforts in working with us and pledge to do our utmost to continue the cooperative teaming relationship that we have established.

Sincerely,



**Nathan Ricks**  
Owner  
STACK Real Estate



**Andrew Bybee**  
Owner  
STACK Real Estate



**Trevor Evans**  
VP, Development  
STACK Real Estate



**Doug Thimm, AIA**  
Senior Principal  
Arch|Nexus



**David Abraham, AIA**  
Principal  
Arch|Nexus

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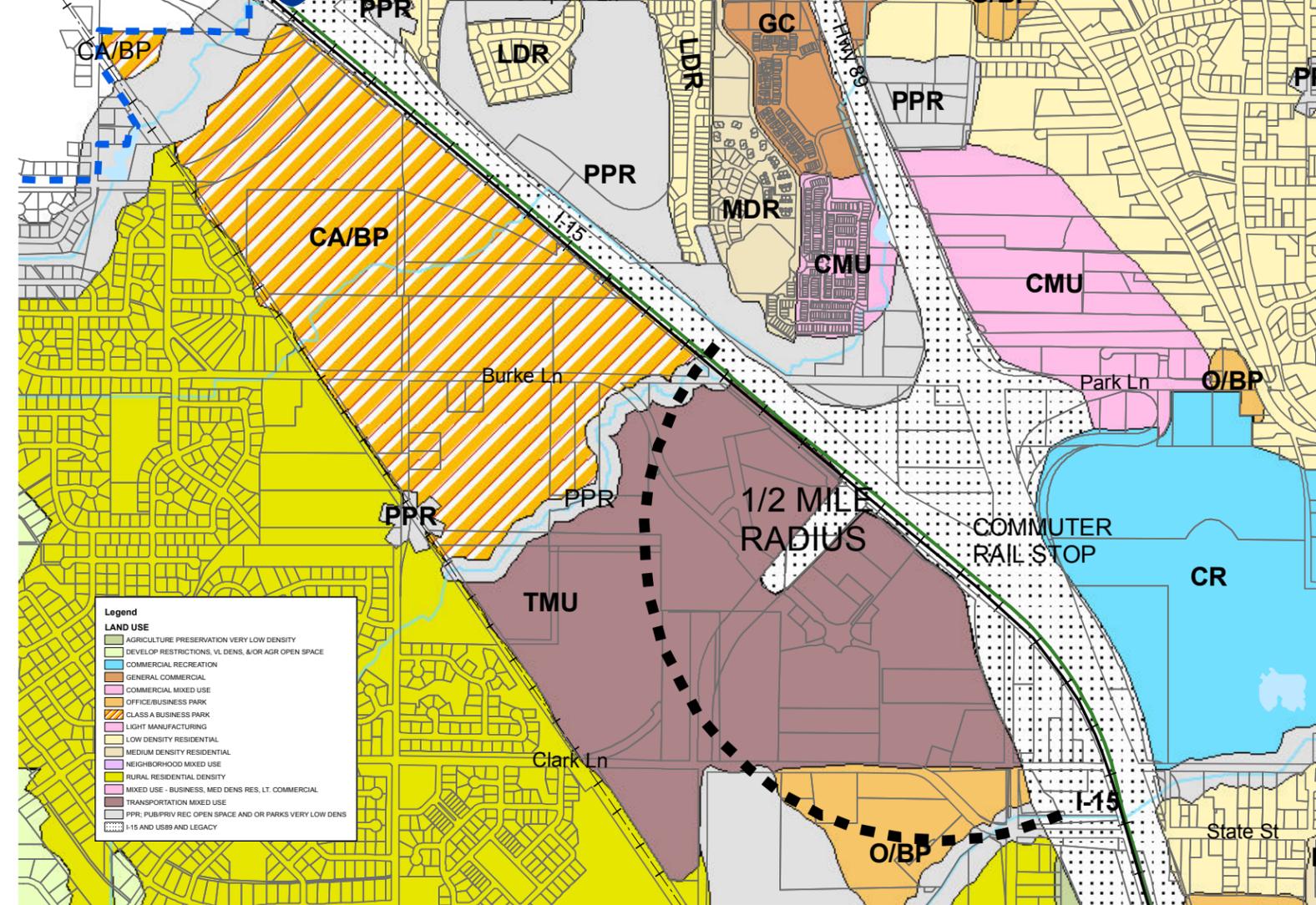
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**GENERAL LAND USE PLAN**  
FARMINGTON CITY



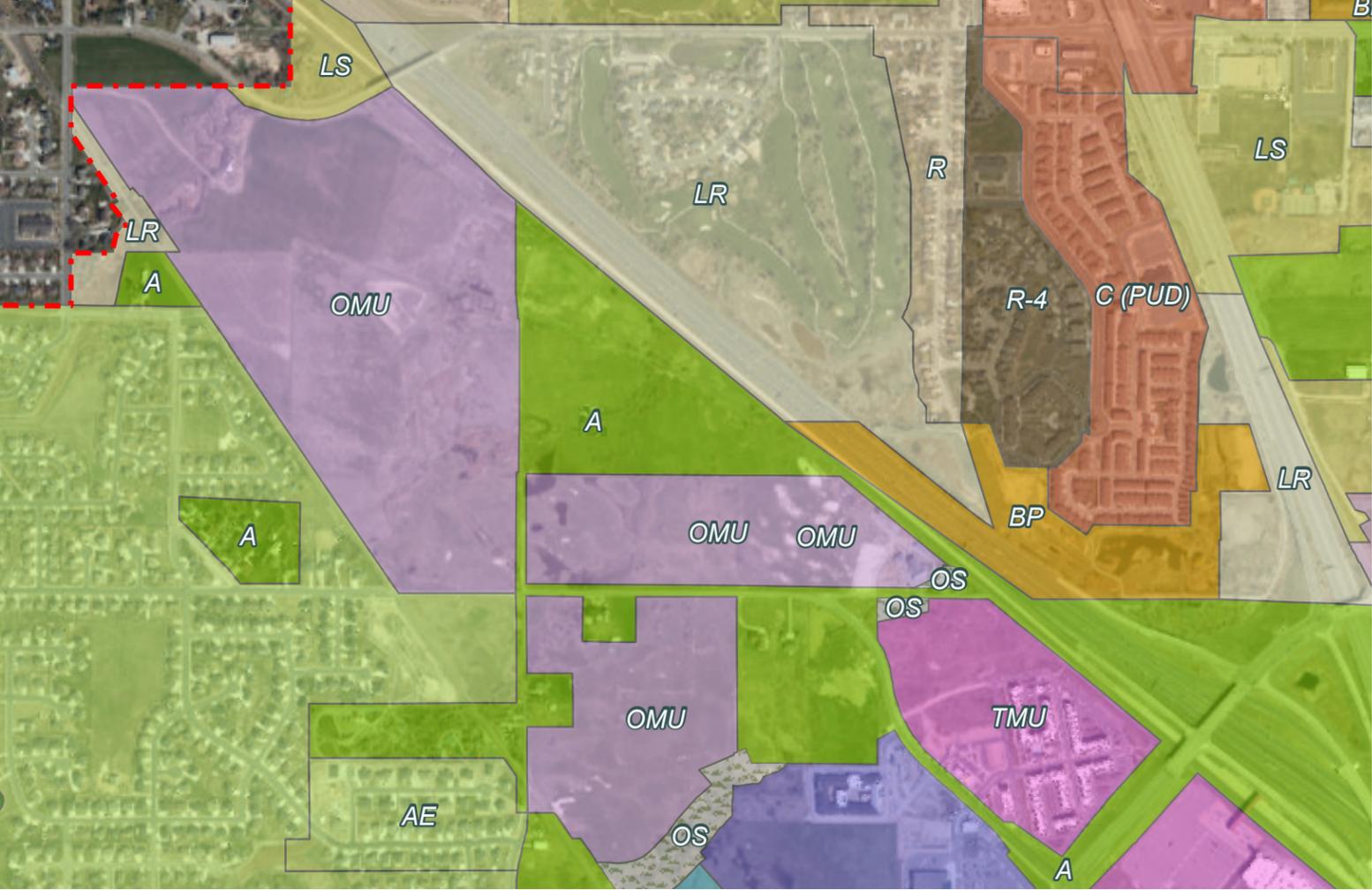
**PROJECT MASTER PLAN NARRATIVE**

**PURPOSE**

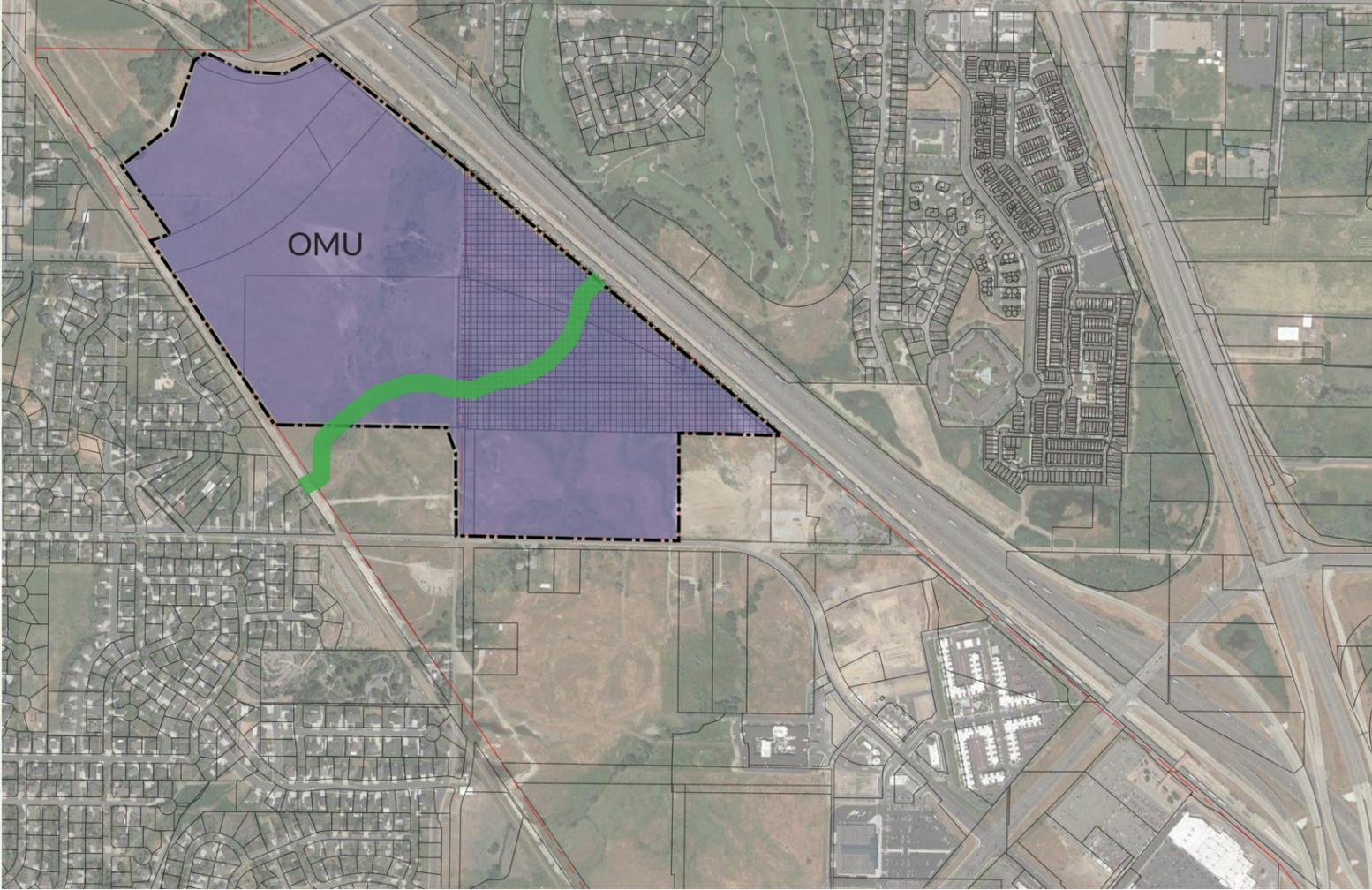
The Mixed-Use Districts Zoning Ordinance (Farmington City Zoning Regulations Chapter 18) establishes development standards and guidelines that are enacted to provide and encourage a compatible mix of uses, rather than a separation of uses, that is consistent with the objectives of the Farmington City General Plan. Flexibility in design and the uses allowed is provided to encourage a diversity of uses that can respond to market forces while being consistent with a design that promotes a transit and pedestrian oriented pattern of development.

The Farmington City General Plan establishes this as a “Class A Business Park”.

We agree with this and are supportive of the City's intent to bring the OMU Zoning to this area, along with developing this as a true Transit Oriented District:



FARMINGTON ZONING MAP



PROPOSED REZONE MAP

-  PROPERTY TO BE REZONED BY DEVELOPER
-  OS ZONE - 50'-0" EACH SIDE OF CENTERLINE OF CREEK. AS COORDINATED B FARMINGTON CITY.
-  OMU Zone

The intent of this PMP is to petition for OMU Zoning for the entirety of the area included within the PMP, which is not now already zoned as OMU:

**Office Mixed Use District (OMU):** The OMU district is intended to be primarily office and commercial. It includes commercial uses appropriate for high visibility locations such as general office, campus uses, and employment centers near collector or arterial streets. The purpose of the district is to encourage office uses in general, allow for a higher intensity of commercial uses than in the RMU, spatially define streets, encourage higher site and building standards, and create an attractive pedestrian environment. Uses that are incompatible with this purpose, including auto related uses, such as repair shops, and industrial uses are not allowed.

**REZONE PETITION**

The majority of the property has already been rezoned as OMU with the remainder to be rezoned to OMU and OS (Open Space) as indicated in the Proposed Rezone Map, from the existing Agricultural (A) Zoning.

## OBJECTIVES OF THIS TRANSIT ORIENTED PMP:

The objectives of this PMP is to align with the Vision and Purpose of Farmington City and the Development team. This is a TOD site and with that goes the ability to bring forth the precepts of successful Transit Oriented Development:

### A. Create an exciting destination

This PMP is all about creating an exciting destination. And, actually this District has already established itself as a Regional Destination. The land area of this PMP fosters the continuance of a vital TOD Site. Care is being taken to create a vibrant and well-connected community featuring employment opportunities along with the necessary residential units to support this type of development; as well as providing usable open space environments and commercial venues intended to draw people from other areas.

### B. Create a complete community

This Mixed-Use Transit Oriented PMP fosters a healthy, walkable and sustainable district, which knits into the community neighborhood fabric providing commerce (restaurant and retail opportunities) along with public open space for both passive and active use.

### C. Provide community assets

This PMP embraces the community with open space amenities along with setting aside land for natural Creekside areas and trail systems including Transit Connections and access to other site amenities intended for the use of residents and visitors.

### D. Promote quality urban design

Our vision is that of continuing the development of a “Great Place” with a sensitive urban solution to land use, integrating complete streets and a pedestrian/bike network including generous landscaping based on an indigenous planting material pallet along with high quality and sustainable architecture. The plan incorporates a town square plaza and park areas as part of the urban core providing a visible and convenient place for gatherings and activities.

### E. Connect the site to the city and region

This PMP intentionally establishes a safe and healthy solution for residents and office users to circulate thru-out the district via an urban street network and connecting pedestrian/bike pathways. The connectiveness of the urban plan provides for accommodation for transit users connecting to the FrontRunner and bus transit options via a direct shuttle between Remote Transit Station at the Village Core and UTA FrontRunner Station.

### F. Promote the City’s heritage

The architectural language of the project is intended to be composition of buildings expressing the heritage of Farmington including materiality and proportion of building size and scale as provided for in the precedent imagery, while also being composed of sustainable contemporary elements. Of course, there will be different scales of development throughout the site that will serve to establish a contextual randomness of character. The materials will include Farmington Rock, brick, concrete, metal panel, weathering steel, wood, along with limited amounts of stucco and cement siding. The buildings themselves will be designed with the traditional elements of defined bottom/middle/top elements of composition.

The employment center office area will be amenitized by active and passive areas and uses that support the workforce that will live, work, and play at North Farmington Station. Open space and connections will be included connecting the office areas to the rest of the site.

## DESCRIPTION OF LAND USE CONCEPTS

The Land Uses included within this Project Master Plan (PMP) are thoughtfully planned with the intention of continued growth of the Transit Oriented Development (TOD) that began with Station Park and has continued to this point. The Land Uses are compatible with both the Farmington City General Plan and the OMU District. The uses include:

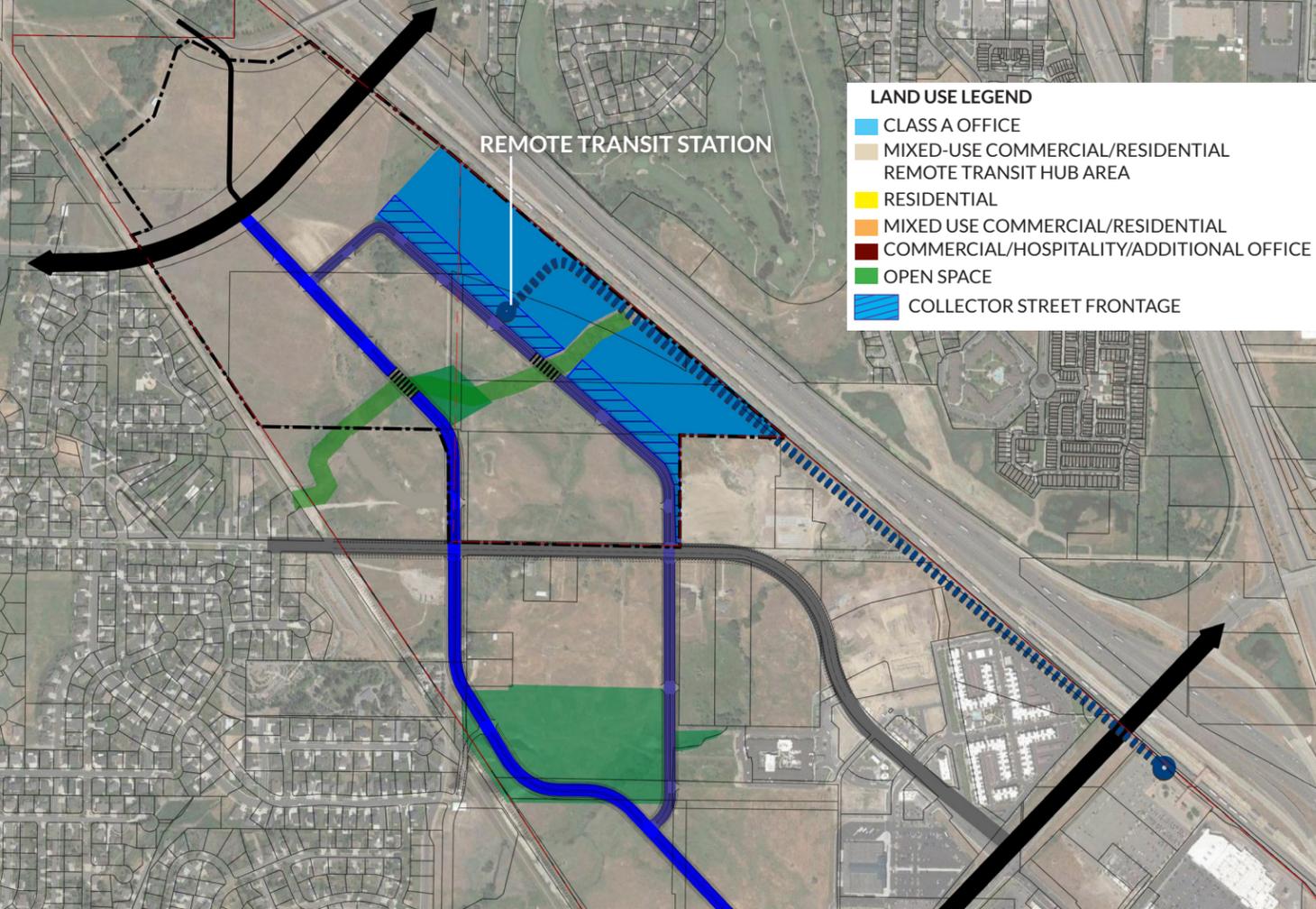
- *Class A Office*
- *Commercial/Hospitality/Additional Office*
- *Mixed-Use Commercial/Residential*
- *Mixed-Use Commercial/Residential Remote Transit Hub Area*
- *Residential*
- *Open Space*

NORTH FARMINGTON STATION CONCEPTUAL LAND USE AREA TAKE OFFS	
July 1, 2020	
Use	Approx. Acres
Class A Office	29
Commercial/Hospitality/Additional Office	27
<b>Subtotal Mixed Use &amp; Non Residential Use Area</b>	<b>56</b>
Mixed-Use Commercial/Residential	14
Mixed-Use Commercial/Residential Remote Transit Hub Area	27
Residential	9
<b>Subtotal Mixed-Use &amp; Residential Use Area</b>	<b>50</b>
<b>Total</b>	<b>106</b>

Note: Conceptual Land Use Areas are approximate and subject confirm based on an ALTA survey

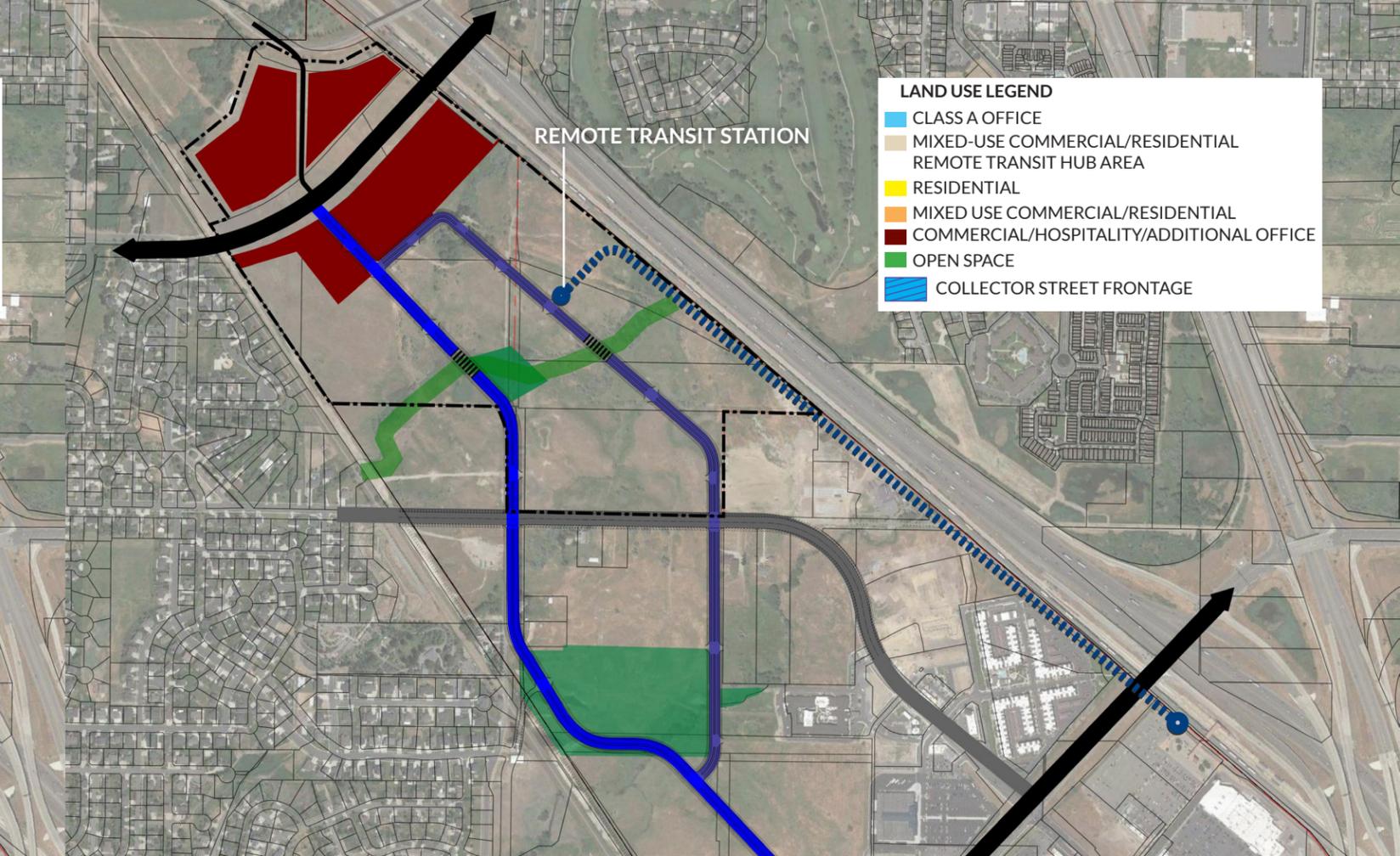
<b>Roads</b>	<b>21</b>
<b>Open Space</b>	OPEN SPACE WILL BE COMPLIANT WITH OMU ZONING (sec 11-18-060) <i>This includes 6 acres of creekside open space</i>

The Planned Uses are intended to create a healthy and walkable continuation of the District in development of continuity and purpose of placement, in developing a fabric of synergistic elements. The master planning has worked within the framework of Farmington City’s Mixed-Use Districts Zoning Ordinance and the City’s General Plan. The basic premise is creating a holistic solution in proximate development of:



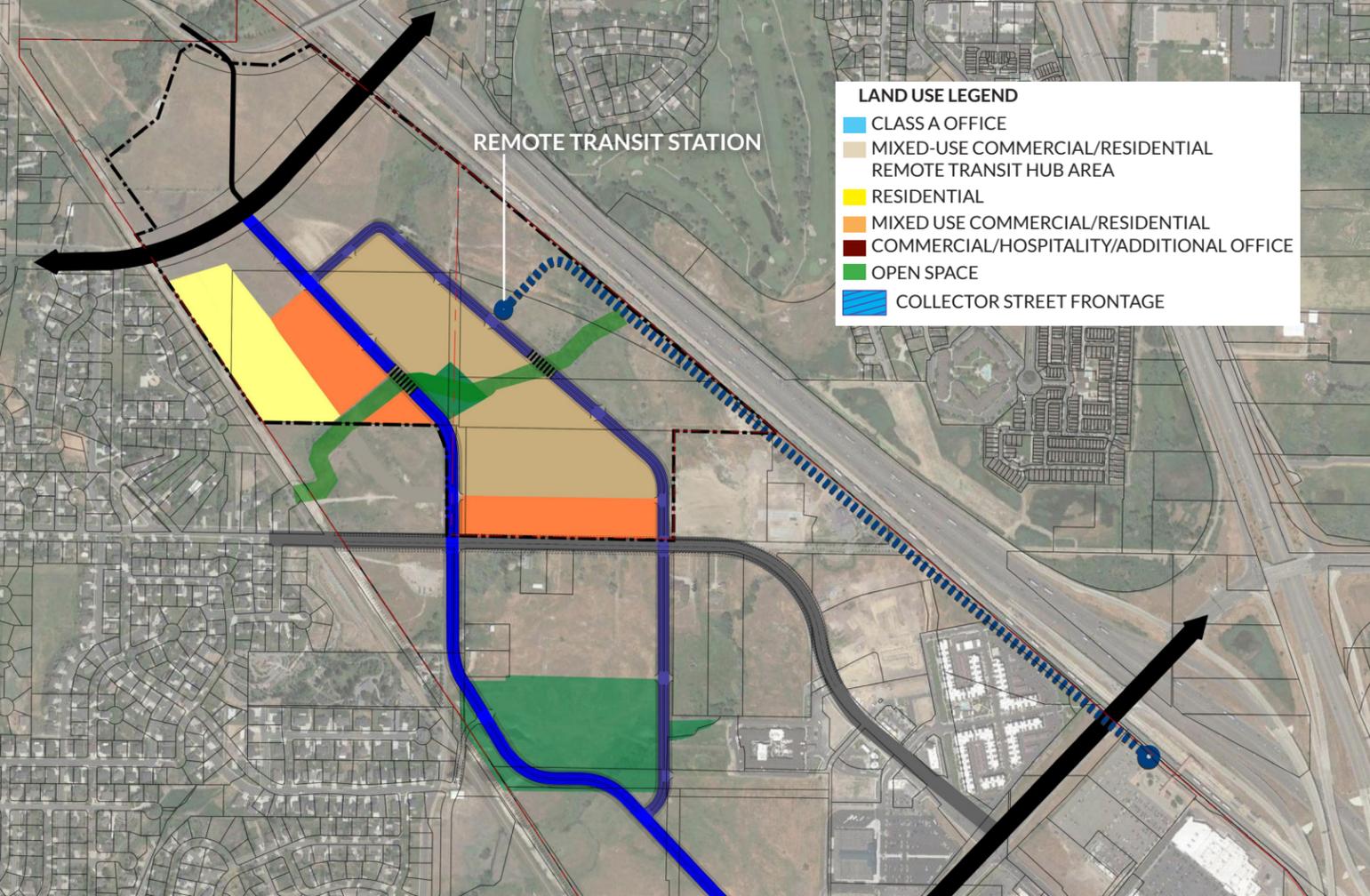
**CLASS A OFFICE**

**Class A Office** Employment Opportunities that are, by visual necessity, located along the Interstate 15 frontage and take advantage of a new Remote Transit Station that is intended to extend Farmington UTA FrontRunner Station's reach into the core of a Class A "Tech Centered" Office Development. Situating this at a transportation station will cause vitality and will provide Farmington City with a sustainable district for decades to come.



**COMMERCIAL/HOSPITALITY**

**Commercial/Hospitality** is a logical extension of Transit Oriented Development. Miscellaneous support commercial uses provide for the establishment of the services and needs that are essential to the district. Of specific note with respect to the business of doing business in a Class A Office environment is the establishment of hotel accommodations, professional office opportunities, multi-story self-storage, neighborhood services, and additional Class A office per market demand.

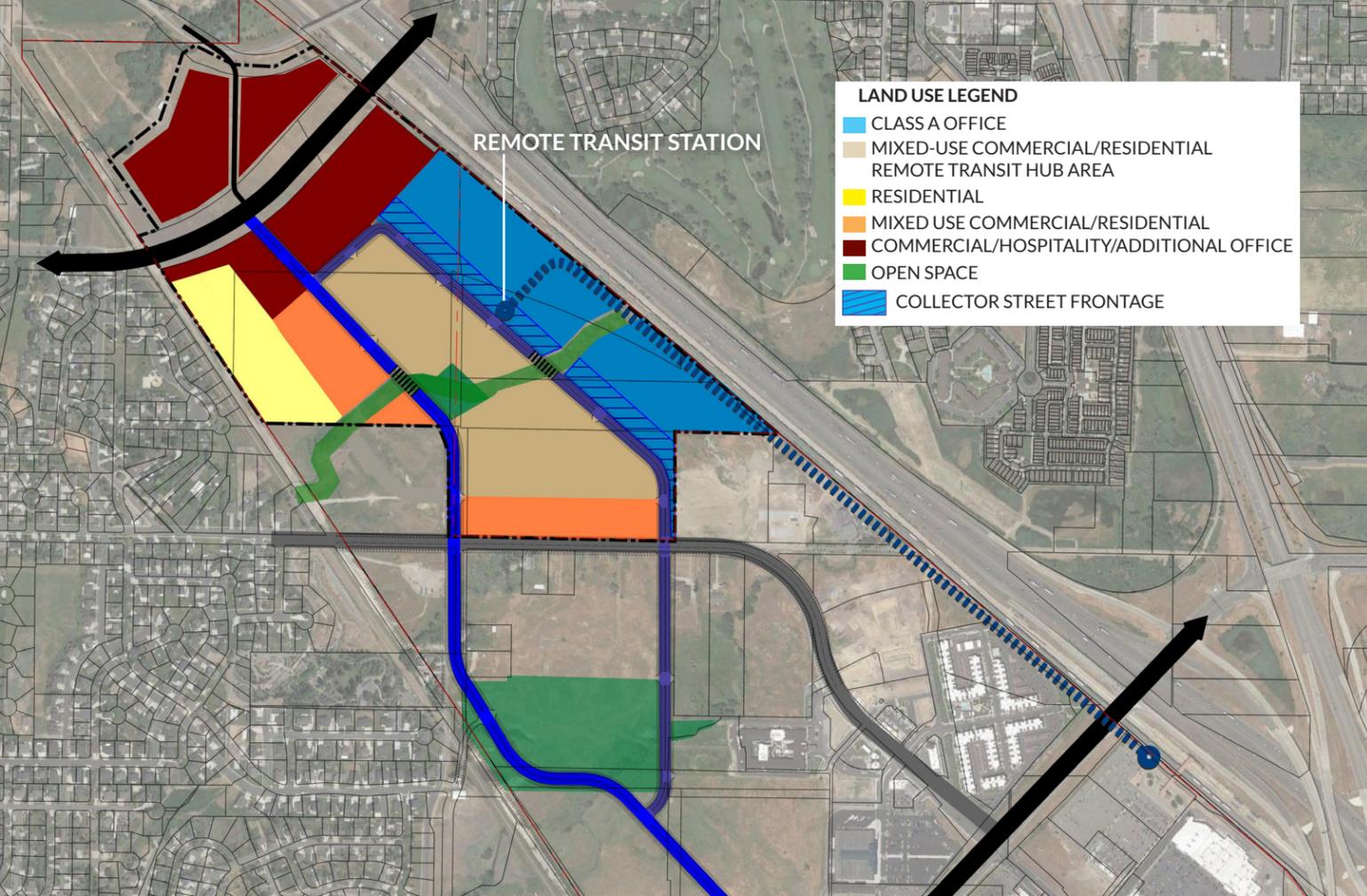


**MIXED-USE COMMERCIAL/RESIDENTIAL**

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**Mixed Use Commercial/Residential** is an absolute necessity to the sustainability and viability of the district. The infusion of housing within a TOD is what brings the neighborhood vitality. This must be more than a “9-to-5 office park”. To thrive, it must be a 24-7 community with real places for people to live. It will be important to provide for a variety of housing types that will appeal to people of differing needs and income levels. Commercial development is intended to enliven this part of the district and create a walkable complement to the regional draw of Station Park. The idea is to create the energy and and life that results form having restaurants, shops, and services in close proximity to Class A Office and complementary to residential development. With this in mind, the residential offerings include:

- *Mixed-Use Commercial/Residential Remote Transit Hub Area*
- *Mixed Use Commercial/Residential*
- *Residential 36 Feet Maximum*



**LAND USE LEGEND**

- CLASS A OFFICE
- MIXED-USE COMMERCIAL/RESIDENTIAL  
REMOTE TRANSIT HUB AREA
- RESIDENTIAL
- MIXED USE COMMERCIAL/RESIDENTIAL
- COMMERCIAL/HOSPITALITY/ADDITIONAL OFFICE
- OPEN SPACE
- COLLECTOR STREET FRONTAGE

**LAND-USE PLAN**

**LAND USE DEVELOPMENT PARAMETERS**

1. The Class A Office Land Use area will be limited to office and parking structure uses only with the exception of the Principal Road Collector Street Frontage which may include Mixed Use Commercial/Residential to screen structures or to create a Mixed-Use environment as indicated by the Land Use Plan.
2. Building Heights by Land Use Area:
  - *Class A Office: 5-Stories minimum*
  - *Mixed Use Commercial/Residential Remote Transit Hub Area: 3 Stories Minimum*
  - *Mixed Use Residential: 3 Stories Minimum*
  - *Commercial/Hospitality/Additional Office: Per Farmington Zoning, Title 11, Chapter 18*
  - *Residential: 36 feet maximum*

**PARKING STRATEGY**

The City's Zoning Ordinance establishes the parking basis for Farmington City in Chapter 32. The minimum parking requirements are based on the Uses served:

- *Office: 3 parking stalls per 1,000 sf of floor area*
- *Multifamily: 1.6 stalls per unit and .25 guest stalls per unit*
- *Commercial: 4 stalls per 1,000 sf of floor area*
- *Restaurants: 12 stalls per 1,000 sf of floor area*

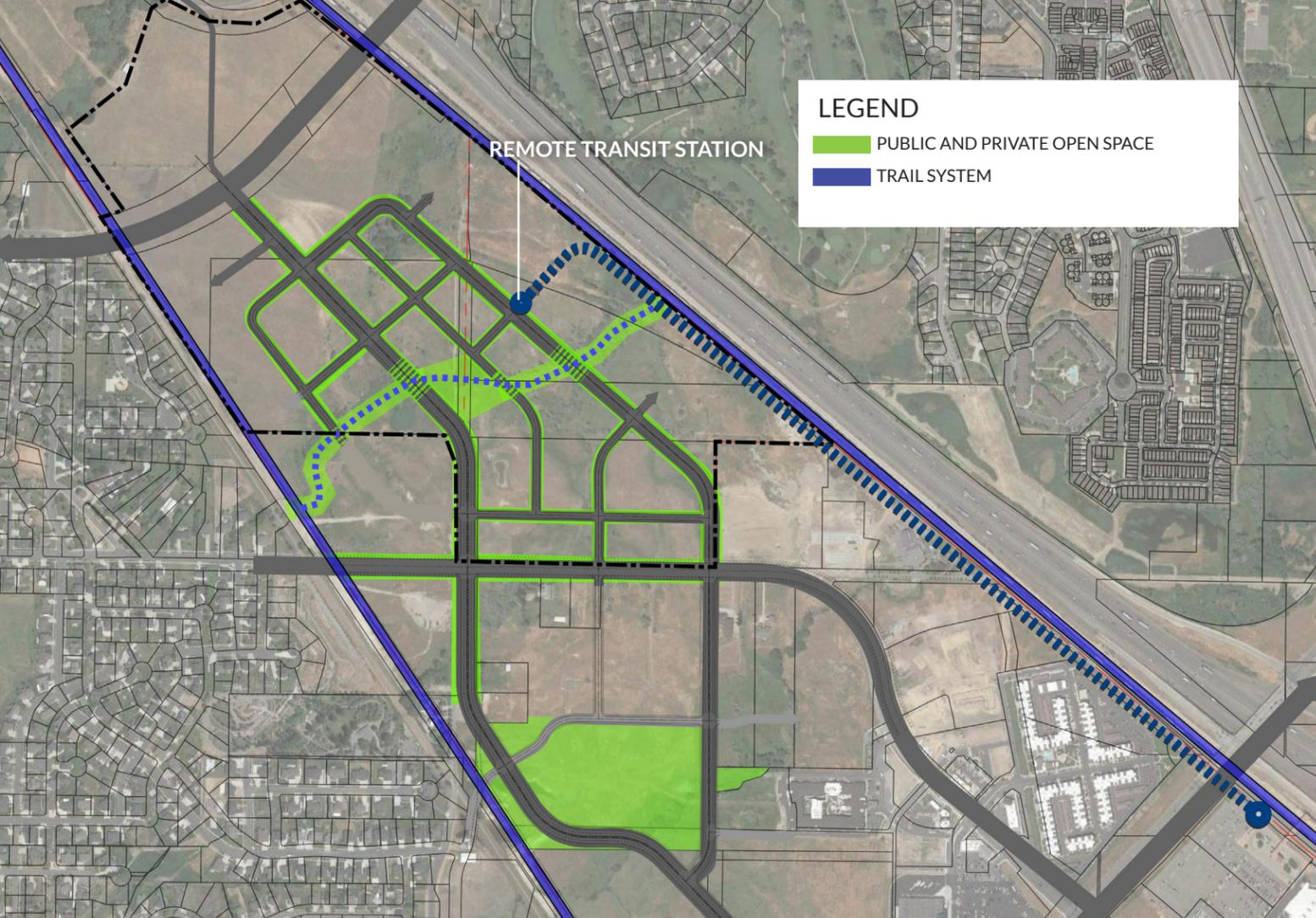
The City's Mixed-Use Ordinance Chapter 18 further defines the parking requirements for Transit Oriented Development, and minimizes the parking rate Transit Oriented Development based on proximity to the Transit Station. With the inclusion of the proposed location of the Remote Transit Station on this site, the minimum parking ratios are significantly reduced:

	Within 1/8 Mile Of Transit Station	Within 1/4 Mile Of Transit Station	Within 1/2 Mile Of Transit Station
Office	50%	40%	25%
Retail/commercial	50%	40%	25%
Residential	40%	25%	15%
Civic/public	50%	40%	25%

It is important to have the right amount of parking for the uses within the PMP. Proper planning should preclude both not enough parking as well as too much parking. The Zoning Ordinance provides for reductions based on shared parking analysis that may be implemented in order to take advantage of complementary uses for further reduction. The fulfillment of this PMP will include a parking study and shared parking analysis if any further reduction to the required parking is to be implemented on a project specific basis. The actual parking that will be provided will be within these parameters as a minimum, with the caveat that the market and the needs of the particular user will drive the final number of stalls provided.

**EXISTING CONDITIONS AND NARRATIVE**

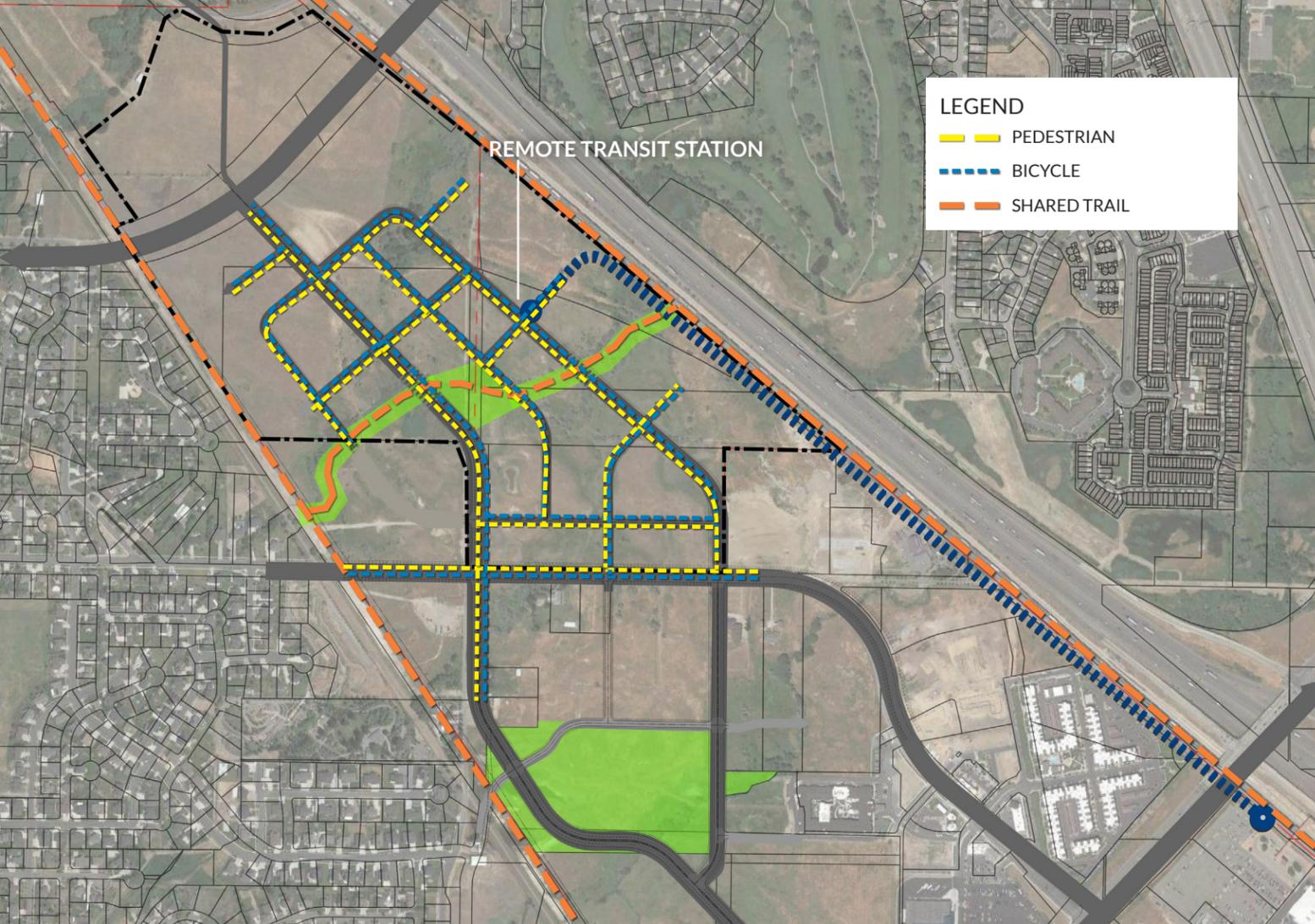
The existing conditions of the PMP area are primarily that of properties that have been under the ownership of multiple owners over the years. The property is now vacant; however, in years past, the usable portions have been utilized for both farming and pasturing. In addition to the "usable land", Shepard Creek passes thru the land from east to west and there are some associated wetlands that have been identified throughout the property. Also, there are some underground utilities in areas of the property and overhead power lines occur along the Interstate 15 frontage area. As development becomes imminent, the property will be fully surveyed and wetlands will be delineated in a proper fashion.



PUBLIC AND PRIVATE OPEN SPACE CONCEPT PLAN



ON-SITE AUTOMOBILE CIRCULATION

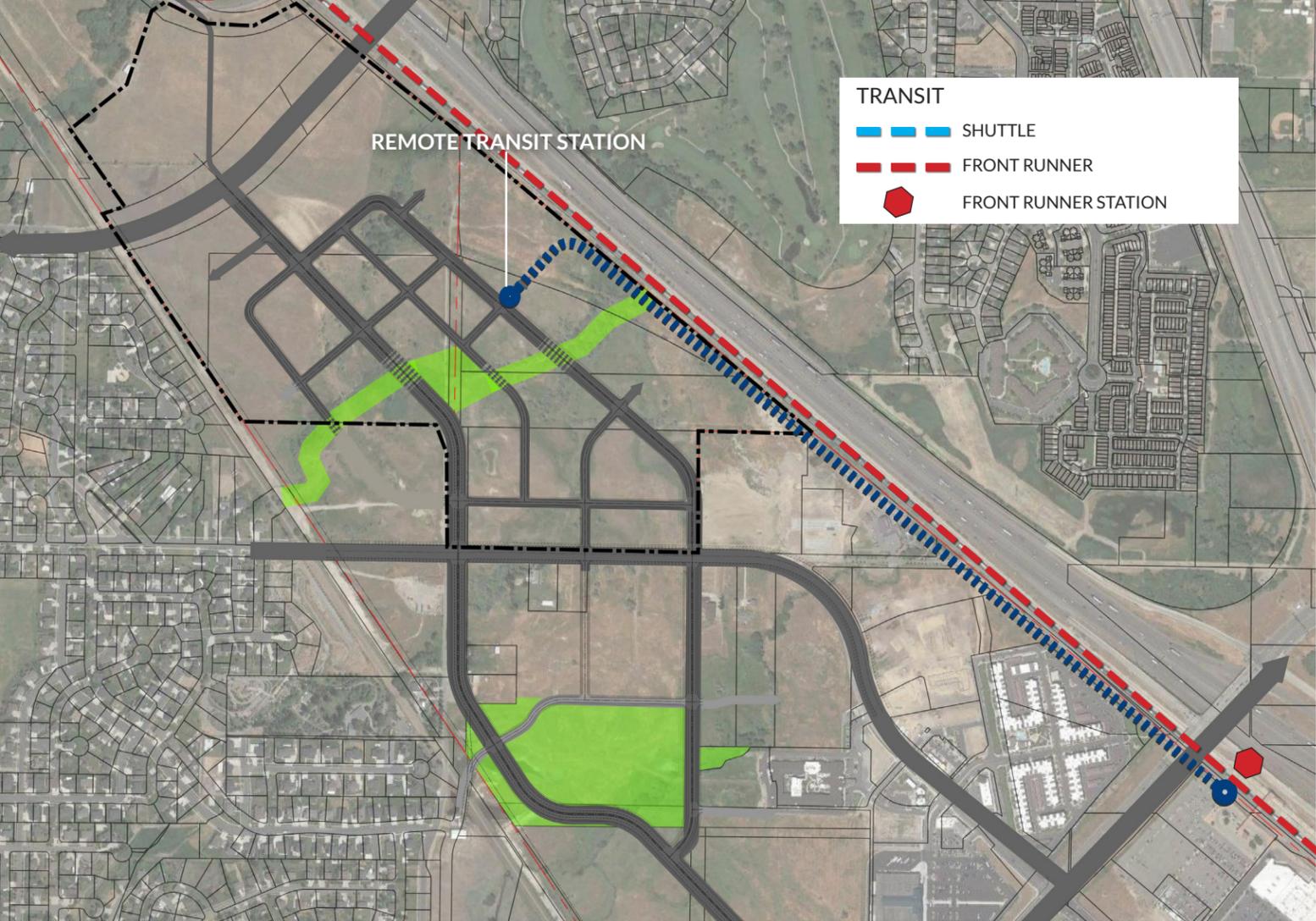


PEDESTRIAN AND BICYCLE CIRCULATION

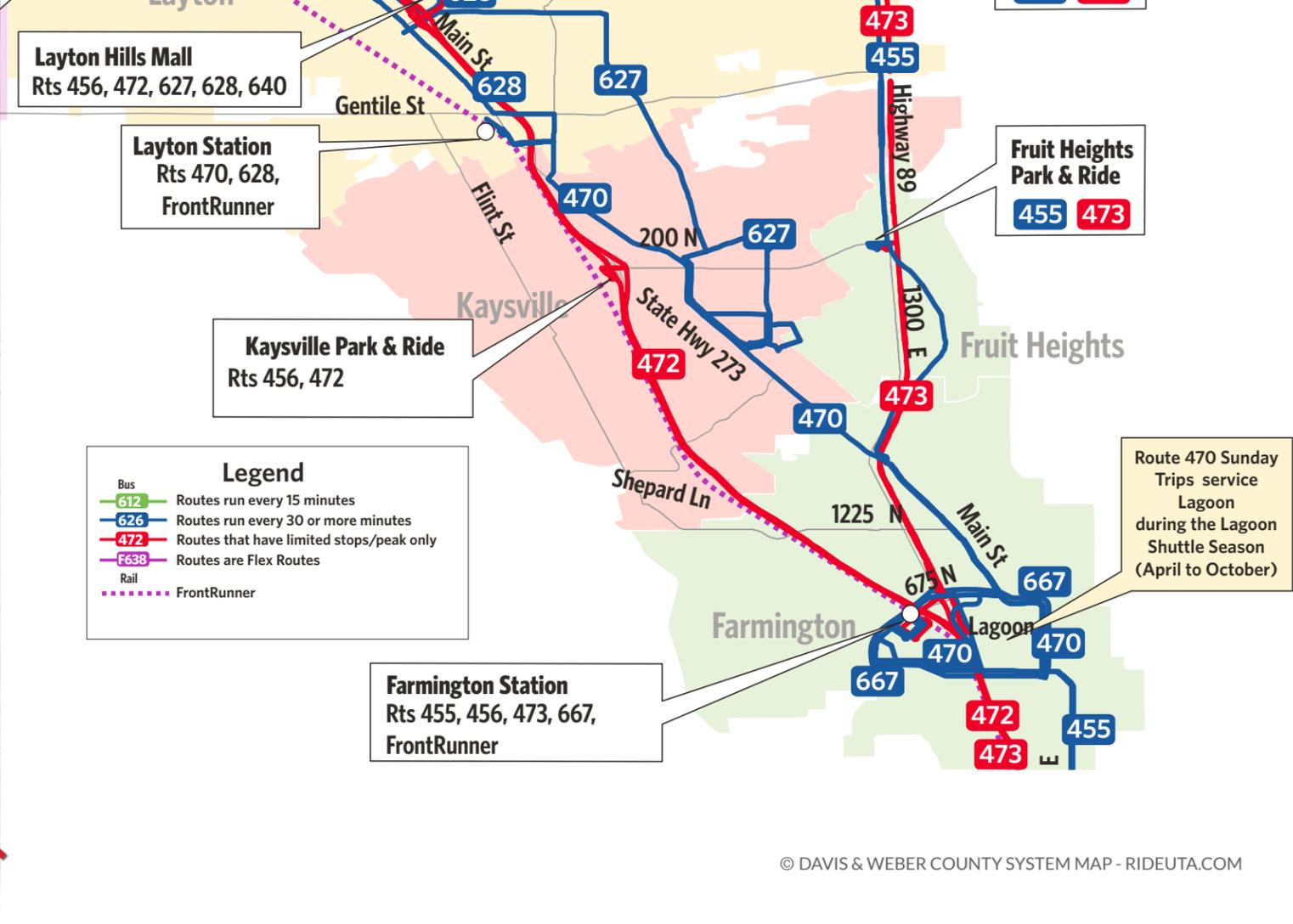
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© FARMINGTON ACTIVE TRANSPORTATION PLAN - 2016



TRANSIT CONNECTIONS



TRANSIT CONNECTIONS



## EXISTING CONDITIONS

### INCORPORATION OF EXISTING STRUCTURES

The PMP area is essentially vacant and has been so for a number of years. There are a few outbuildings on the property that appear to have no significant historic value and have outlived their usefulness. As the property is developed, these structures will be removed in accordance with the requirements of the Farmington City Building Department.

### DEVELOPMENT STANDARDS AT EDGE OF THE PMP AREA

The 127-acre PMP area extends from Haight Creek and the existing Shepard Creek right-of-way bounded by primarily Haight Creek at the north, Interstate 15 on the east, Burke Lane on the south and the Denver and Rio Grande Western Rail Trail on the west. The uses proposed by this PMP are compatible with the uses on the adjacent properties

### NORTH EDGE

The north boundary of the PMP area is defined by Haight Creek and the existing Shepard Lane Right of Way. It is anticipated that Haight Creek will be left in its natural state and will ultimately be zoned as Open Space as depicted by the General Plan. The area to the north of Haight Creek and Shepard lane is largely open space and greenbelt at this time, but does include the Water District Building along with another barn-like structure. The proposed uses at this edge of the PMP area include Commercial/Hospitality which will be buffered from any use that occurs further to the north by the Haight Creek Open Space Area.

### EAST EDGE

The east boundary of the PMP is defined by the Interstate 15 corridor along with major Rocky Mountain Power transmission lines. The transportation plan for the area makes provision for an extension of the Legacy Trail system that borders the Interstate 15 right of way. The proposed uses at this edge include expansion of the trail system to include a dedicated bus shuttle from the new Remote Transit station in the PMP, while maintaining the pedestrian/bike connectivity of the Legacy Trail extension. The proposed development at this edge of the PMP area is 5-story and higher Office Building and associated parking structures.

### SOUTH EDGE

The south boundary of the PMP is defined primarily by Burke Lane for the easterly 2/3's of the area towards the west and developable land to the east. The existing development to the south of Burke Lane includes a few single-family dwellings on large lots that are well set back from Burke Lane and the Red Barn Development. The City's General Plan calls for this area to ultimately be rezoned to OMU zoning (part of which has already happened) with a development pattern that is consistent with that which is proposed by this PMP. For the near future, prior to rezoning and redevelopment, the existing homes are well setback from Burke Lane affording a generous buffer from the development that is planned by this PMP, which is Mixed-Use Commercial/Residential to the west and Class A Office to the east.

### WEST EDGE

The west boundary of the PMP is defined by the south and the Denver and Rio Grande Western Rail Trail and a utility easement. The existing development to the west of the Rail Trail is comprised of lower density single family development. The City's General Plan calls for this area to be rezoned to OMU zoning (which has already occurred for most of the property) with a development pattern that is consistent with that which is proposed by this PMP. The development that is planned by this PMP provides for Low Density Residential Mixed-Use (36 feet maximum height) use directly adjacent to the Rail Trail and observing a utility easement on the east side of the Rail Trail. The Low-Density Residential use proposed by this PMP has been thoughtfully placed as a transition between the Low-Density neighborhood and more intensity of use to the east.

## SEQUENCE AND TIMING

The sequence of development actually begins with municipal improvements, some of which are underway and others, which are planned to start in the near (within 12 month) term. These include:

- *Construction of Rights of Way including Burke Lane – now under construction.*
- *Development of the City's regional detention facility. This includes the placement of new storm water pipes extending to the north, which may serve (in part) the area of this PMP.*
- *Extension of the Central Davis sanitary sewer system, which may serve (in part) the area of this PMP*
- *Public roadway construction*
- *Etc*

The full development of the PMP will be a process that plays out over many years in multiple phases. Some of the work will begin and occur concurrently with the municipal improvements and some of which depend on municipal improvements to be completed, including::

- *Property survey of the PMP area*
- *Delineation of any wetlands within the PMP area*
- *Subdivision plats including Right of Way dedications for city roads and utilities*

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## MEMORANDUM

Date: May 28, 2020  
To: Farmington City  
From: Hales Engineering

Subject: **Farmington Stack TOD TIS**



UT19-1601

This memorandum discusses the regulating plan submittal completed for the Farmington Stack TOD project. Included in this document is an analysis of the anticipated trips generated by the development, reductions to traffic based on internal capture, transit, and active transportation, and an analysis of the proposed roadway network.

### Project Description

The proposed development is anticipated to contain the following land uses:

- Residential buildings
- Retail space
- Hospitality
- Gym/fitness center
- Grocery store
- Convenience market
- Office buildings
- Flex office space

These land uses were taken into consideration when estimating the number of trips that would be generated by the site.

### Trip Generation

Trip generation for the development was calculated using trip generation rates published in the Institute of Transportation Engineers (ITE), *Trip Generation*, 10<sup>th</sup> Edition, 2017, and were modified with the adopted EPA methodology.

The trip generation for the proposed development by 2050 is as follows:

- Daily trips: 25,626
- Morning peak hour trips: 1,902
- Evening peak hour trips: 2,317

### Mode Split

Because the project is a transit-oriented development, it was assumed that some conservative reductions could be made to reduce the number of trips generated by the site. A 7 percent transit reduction was made based on the development's proximity to the nearby FrontRunner station. This reduction was limited, however, by the fact that the project is located greater than a quarter of a mile away; however, the project is planning to provide a circulator shuttle to move people between the FrontRunner Station and the office buildings.

An alternative transportation mode reduction was also made because many trips will likely be made by people walking, biking, riding a scooter, etc. between the office buildings and the residential components etc. An estimate of 16 percent was utilized based on the EPA methodology.

### Internal Capture

The mixed-use nature of the development also allows for reductions to trip generation. Because retail and office spaces are located in close proximity to denser housing, it is likely that many people will choose to walk within the development to work or to shop. A 16 percent internal capture reduction was made to the daily trips and an 18 percent reduction was made for peak hour trips, based on the EPA internal capture methodologies.

### Roadway Network

Following completion and preliminary approval of the trip generation from the City, Hales Engineering will distribute trips from the project to the proposed roadway network, see Figure 1.

If you have any questions, please feel free to reach out to us.

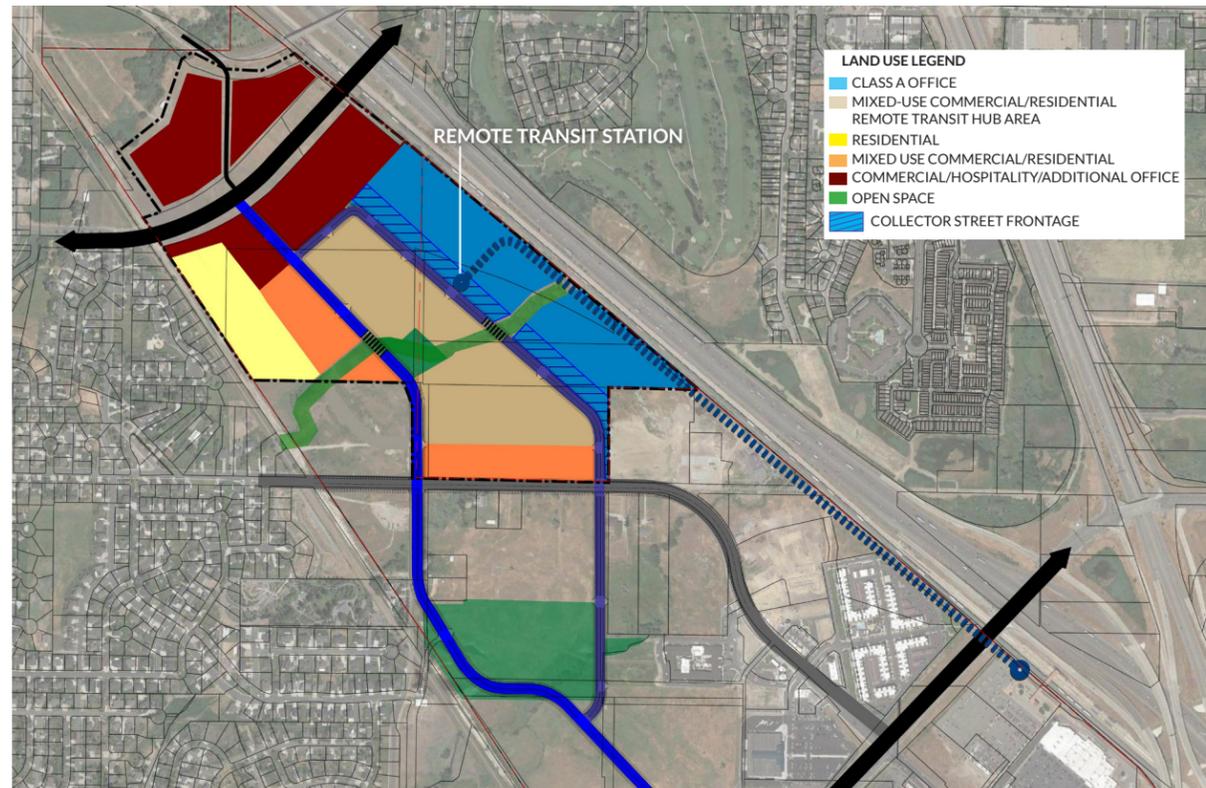
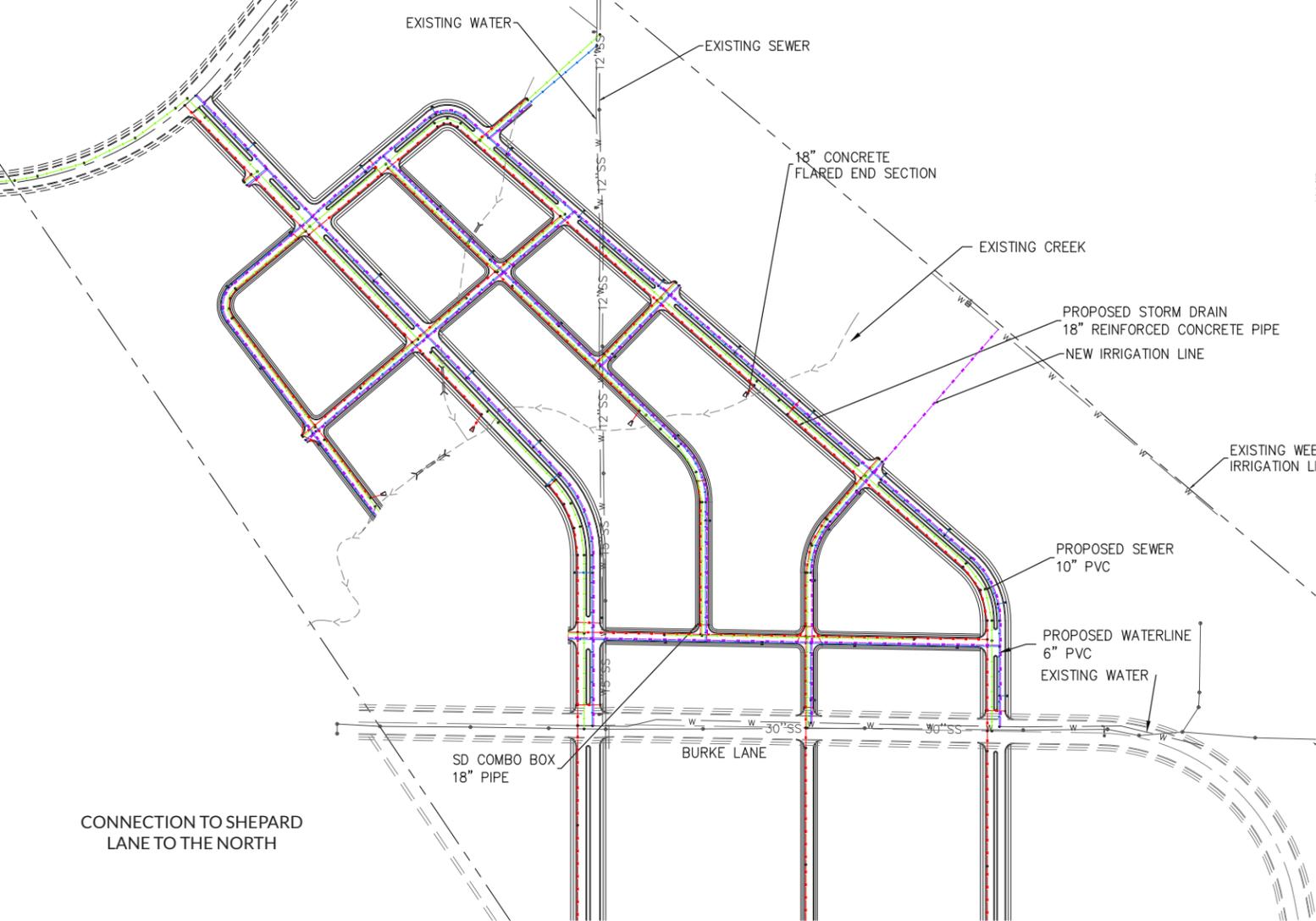


Figure 1: Proposed Roadway Layout

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CONNECTION TO SHEPARD LANE TO THE NORTH

**COMBINED UTILITIES**

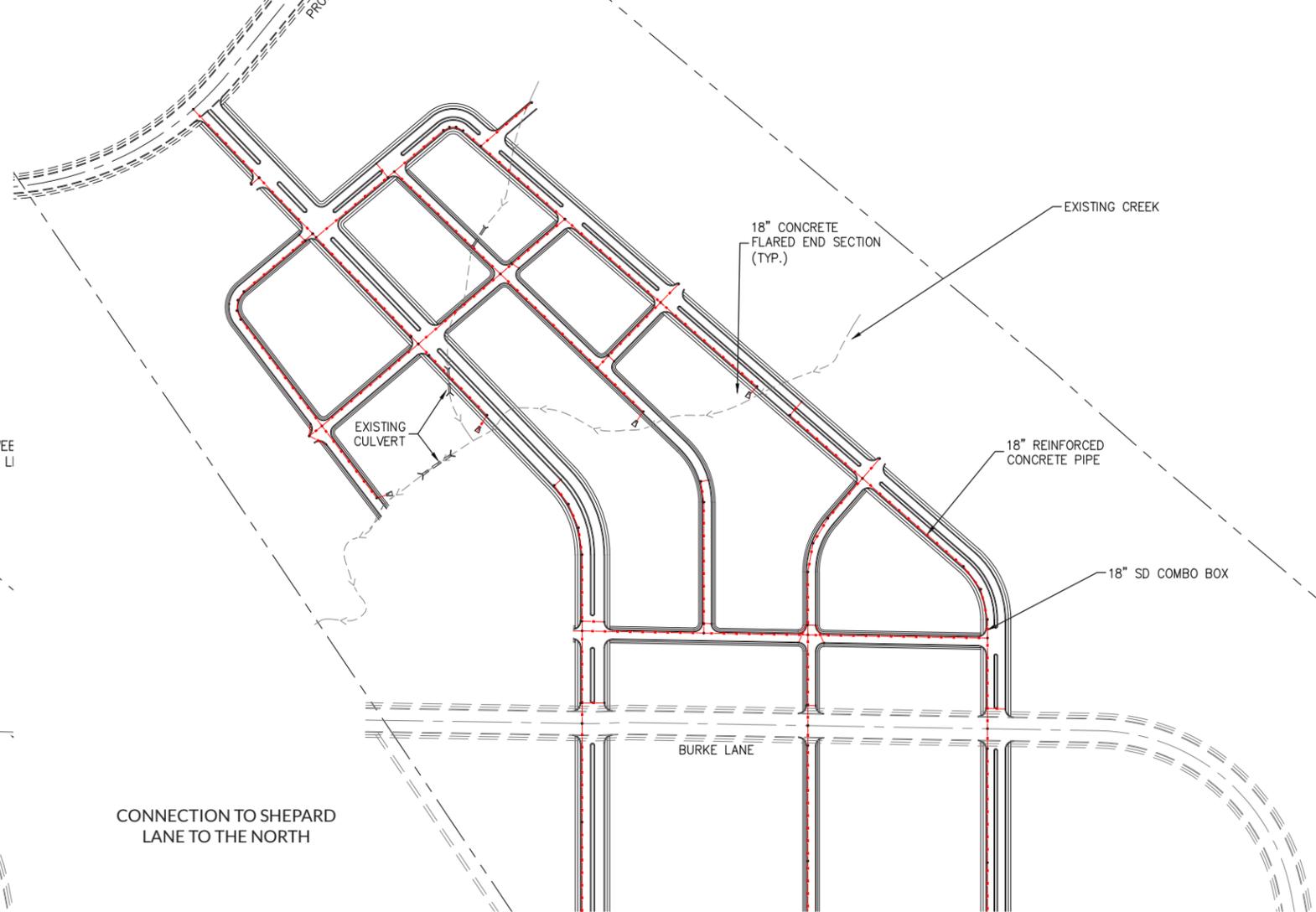
**UTILITIES AND INFRASTRUCTURE**

The following is a brief description of the existing and proposed infrastructure that will serve the new Farmington Station development. Detail will be provided on the culinary water system, irrigation water systems as well as sanitary sewer and storm drain. Also, a brief discussion of the dry utilities to serve the site is provided below.

**DRY UTILITIES:**

Dry utilities for the project are available in the area and are being coordinated with the various providers including Dominion Gas, Rocky Mountain Power, Utopia, Comcast and several others. Dominion Gas has a high-pressure line that exists in an easement along the rail/trail corridor. Other utilities are present to the south and will be extended to the project to provide service.

LEGEND	
	PROPOSED WATER
	PROPOSED SEWER
	PROPOSED STORM DRAIN
	EXISTING WATER
	EXISTING SEWER
	EXISTING CREEK
	PROPOSED CURB & GUTTER



CONNECTION TO SHEPARD LANE TO THE NORTH

**STORM DRAIN**

**STORM WATER:**

Stormwater for the project will be separated into 3 distinct and separate individual areas. Each area will be detained and routed to separate discharge locations located throughout the project per discussions with Chad Boshell, City Engineer for the City of Farmington.

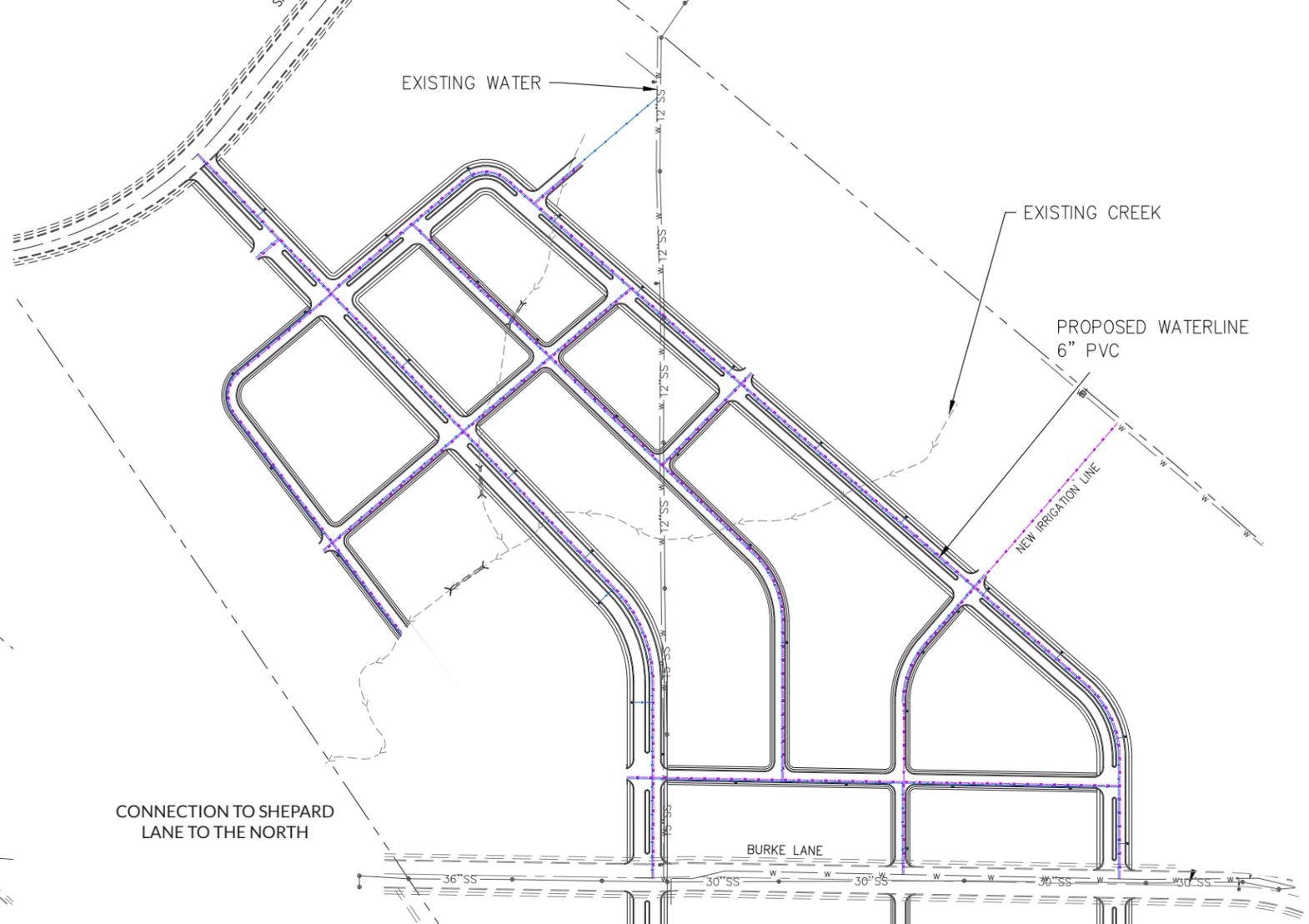
The first drainage basin is located to the North of Sheppard Lane as shown on. Drainage from this parcel will be required to detain on site flows and released at 0.2 CFS per acre. Water will be discharged into an existing culvert located along the rail/trail corridor on the West edge of the property. Stormwater will be required to be treated to remove pollutants prior to being discharged into the city system.

The second drainage basin will be for the property located South of Sheppard Lane and North of the existing Creek running through the project. These flows will be required to detain at 0.2 CFS per acre release rate and then discharged into the existing Creek after being treated for pollutants. The 3rd drainage basin is for the property located South of the existing Creek and North of Burke Lane. These properties will be required to detain at 0.4 CFS per acre. Post detention flows from this basin will be routed to Burke Lane where they will connect to new 30" pipelines to be constructed by Farmington City that route the flows to the new detention pond and park facility planned for the area. System layout is shown on the figure above.



**SANITARY SEWER:**

Sanitary sewer for the project will be provided by Central Davis Sewer District. The District currently has sewer lines located in both 1525 W and in Burke Lane. The existing line located in 1525 W is a 12-inch diameter PVC sewer that connects to the 30-inch line located in Burke Lane. Both of these lines are currently flowing at capacity and are restricted in accepting new sewer flows. The district plans to run a new collector line from the treatment facility to the edge of the rail/trail corridor at the new Shepard Lane ROW where it will terminate. It will be necessary to connect this new pipeline to the existing outfall at the North End of 1525 W in order to divert flows from the North part of Farmington City to the new collector to be constructed by Central Davis. Once the new connection is made and the outfall line is functional, capacity will be freed up in the existing sewer lines running through the project. The new development will route the bulk of the flow for the project down to the existing 30-inch line in Burke Lane. A portion of the development located on the North section of the property will be routed into the new collector line being constructed as part of this project and as part of the Central Davis new collector project.



**CULINARY WATER**

**CULINARY WATER:**

Culinary water lines exist in both 1525 W St and Burke Lane and are controlled by Farmington City. The project will connect to both of these existing water lines and provide a new culinary water loop throughout the development to provide fire flow and domestic service to the proposed development (see overall utility map for water system map). The existing pipelines for this city are both 10 and 12 inch in diameter and should be ample to supply the proposed development for fire flow and for domestic service. A looped water system will be installed to provide redundancy for the development.

**SECONDARY WATER:**

Irrigation water for the project will be provided by the Weber Basin Water Conservancy District through their existing line located on the East side of the property running from the South to the edge of the existing Creek. All parcels within a development will be required to use Weber Basin water for their irrigation needs. The development will connect to the existing Weber Basin line and provide a loop system throughout to provide the irrigation needs for the development. The system will be designed using purple PVC C 900 pipe for the delivery of the irrigation water. Detailed layout of the system will be done during the design phase.



## SECTION 140 PETITION

Alternative Approval Process; Development Agreements (Section 11-18-140) Petition

The Farmington City Zoning Ordinance makes provision for an Alternative Approval Process; Development Agreements (Section 11-18-140). This makes provision for refinements to Chapter 18 of the Zoning Ordinance in conjunction with a Development Agreement as outlined by Section 11-18-140:

“Consideration and Approval Of Development Agreement: The development agreement shall be considered at the same time as the PMP and following the same approval process described in section 11-18-080 of this chapter. The criteria for review of a PMP and development agreement application by the Planning Commission and City Council shall consist of the following criteria in lieu of the criteria set forth in subsection 11-18-080 of this chapter:

1. Consistency with the Farmington City General Plan;
2. Compliance with applicable City codes, rules, regulations and standards applicable to the proposed PMP, except that uses and development standards specifically included in the development agreement may be different from those contained in the Farmington City ordinances;
3. Consistency with any development standards determined by the City to be applicable to all development within the TOD Mixed Use Districts;
4. Establishment of a mix of uses in locations that will promote and encourage the goals of the TOD Mixed Use Districts and be consistent with the objectives of section 11-18-050, “Uses”, of this chapter; and
5. Establishment of circulation and transportation features sufficient to meet the requirements of section 11-18-040, “Regulating Plan”, of this chapter, to coordinate with anticipated off site circulation and transportation features and to further any applicable community wide transportation objectives.”

The following items are included in this petition:

- The Permitted Residential Uses for this PMP as outlined in the Land Use Plan are to include:
  - Mixed-Use Commercial/Residential
  - Residential
- The Building Height requirements are to be as follows:
  - Interstate 15 Frontage/Class A Office – Minimum Height of 5 stories
  - Mixed-Use Commercial/Residential Remote Transit Hub Area – Minimum Height of 3 stories
  - Mixed-Use Commercial/Residential – Minimum Height of 3 stories
  - Residential - Maximum Height 36 feet
- The Building Siting Requirements are to be as follows:
  - Interstate 15 Frontage will have NO REQUIREMENT for RBR
- The Open Space Requirements are to be as follows:
  - Interstate 15 Frontage will require 10% Open space, which includes the Legacy Trail/Bus shuttle lane width.
- Regulating Plan Modification.

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## Planning Commission Staff Report July 9, 2020

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### **Item 5: Zoning Map Amendment – Rezoning Right-of-Way’s**

Public Hearing:	Yes
Application No.:	Z-8-20
Property Address:	NA
General Plan Designation:	NA
Zoning Designation:	See attached map
Area:	NA
Number of Lots:	NA
Applicant:	Farmington City

*Request: Recommend approval of zoning map amendments concerning the rezoning of City-owned right-of-ways (ROW)*

---

### **Background Information**

The zoning map, is updated after the City Council approves the rezone of a parcel of land in the development process. However, after City Council approval, changes to the zoning map only include the parcels for which the approval was given, or where the plat indicates. Thus, the current zoning map is a jigsaw puzzle where all parcels are surrounded by right-of-ways that have not been rezoned accordingly. This map amendment would allow the zoning map to be amended to rezone City-owned ROW to reflect abutting property zones.

Section 11-9-030 of the zoning ordinance describes the event in which zoning boundary uncertainties exist that the centerline of ROWs will be used as zoning boundaries first. This zoning map would follow this guideline; therefore, the zoning map would eliminate out-of-date ROW zoning and replace those areas with centerline reflections of adjacent zoning.

### **Suggested Motion**

Move that the Planning Commission recommend the City Council approve the zoning map amendment subject to all applicable Farmington City ordinances and standards.

### **Findings:**

1. The amendment allows updates to the zoning map, which is currently updated to reflect just the properties requesting rezoning.
2. This amendment allows for continuity and simpler map viewing.
3. This amendment will allow ROWs in future developments to be updated at the same time as said developments (if rezoning is applicable).

Supplementary Information

1. Existing zone map: to be shown at meeting
2. Proposed changes to the zoning map: to be shown at meeting



## Planning Commission Staff Report July 9, 2020

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### Item 6: Nielsen Detached Garage Conditional Use

Public Hearing: Yes  
Application No.: C-2-20  
Property Address: 1774 West Spring Meadow Lane  
General Plan Designation: RRD (Rural Residential Density)  
Zoning Designation: AE (Agriculture Estates)  
Area: .26 acres  
Property Owner: Ryan and Claurys Nielsen  
Applicant: Ryan Nielsen

Request: *Applicant is requesting a conditional use approval to increase the height of an accessory building (detached garage) from 10 feet to 15 feet in height with a setback of 1 foot from the property line.*

---

#### **Background Information**

The applicant wishes to place a detached garage in the side yard of his lot. The applicant has a triangular-shaped lot and the rear/side lot line abuts a section of the Farmington Ranches Park Trail. (see attached photos) The property is zoned AE and subject to the standards set forth in Chapter 10 (Agricultural) of the zoning ordinance, but because the garage is subordinate in height and area to his home (the main building) and is no taller than 15 feet; he is able to follow the standards for accessory buildings in Chapter 11 (Single Family Residential Zones). Henceforth, the applicant's request requires a conditional use permit from the Planning Commission. See attached Sections 11-11-060 A and 11-11-070 A & B.

#### **Suggested Motion:**

Move that the Planning Commission approve the conditional use application to allow the 15 foot garage to be placed within 1 foot of the property line, subject to all applicable codes, development standards and ordinances and the following conditions:

1. The driveway curb cut cannot exceed the 30 feet max.
2. The building may not exceed 15 feet in height.
3. Building code will allow a structure to be within one foot of the property line, but it does not allow for an eave. Any eave, or part of the accessory building, shall not overhang or extend past a property line, nor drain on a neighboring property. Any wall within 5 feet of the property line requires a 1-hour fire assembly.

#### **Findings for Approval**

1. The use is not contrary to the goals, policies and governing principles of the comprehensive plan for Farmington City.
2. The subject property is large enough that a detached garage will fit on the property without any foreseeable adverse effects and is not detrimental to the health, safety or general welfare of persons residing or working in the vicinity.
3. The accessory buildings is subordinate in height to the main building.
4. The garage will be located at least fifteen feet (15') from any dwelling on an adjacent lot;
5. The detached garage will not encroach on any recorded easement;
6. The proposed structure is compatible with the character of the site, adjacent properties and surrounding neighborhoods.
7. Constructing a detached garage on the property is a right other property owners in the neighborhood have. The triangular shape of the lot limits the location on the lot, which the garage can be placed.

### **Supplemental Information**

1. Vicinity Map
2. Site Plan
3. Elevations
4. Photos (below)
5. Section 11-11-060 A Accessory Buildings and Structures
6. Section 11-11-070 B Building Height
7. Section 11-8-050 Conditional Use Standards

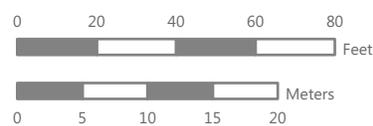
### **Applicable Ordinances**

1. Title 11 Chapter 8 Conditional Uses
2. Title 11 Chapter 11 Section 070 Building Height





**VICINITY MAP**  
**1774 W Spring Meadow Lane**



Disclaimer: This map was produced by Farmington City GIS and is for reference only. The information contained on this map is believed to be accurate and suitable for limited uses. Farmington City makes no warranty as to the accuracy of the information contained for any other purposes.

11-8-050: CONDITIONAL USE STANDARDS:

Conditional use applications shall be reviewed in accordance with, and shall conform to, all of the following standards:

A. Necessity: The proposed use of the particular location is necessary or desirable to provide a service or facility which will contribute to the general well being of the community;

B. Compliance: The proposed use shall comply with the regulations and conditions in this title for such use;

C. Comprehensive Plan: The proposed use shall conform to the goals, policies and governing principles of the comprehensive plan for Farmington City;

D. Compatibility: The proposed use shall be compatible with the character of the site, adjacent properties, surrounding neighborhoods and other existing and proposed development;

E. Adequate Improvements: Adequate utilities, transportation access, drainage, parking and loading space, lighting, screening, landscaping and open space, fire protection, and safe and convenient pedestrian and vehicular circulation are available or may be provided; and

F. Use Not Detrimental: Such use shall not, under the circumstances of the particular case, be detrimental to the health, safety or general welfare of persons residing or working in the vicinity, or injurious to property or improvements in the vicinity. A proposed use shall be considered detrimental:

1. If it will cause unreasonable risks to the safety of persons or property because of vehicular traffic or parking, large gatherings of people, or other causes;
2. If it will unreasonably interfere with the lawful use of surrounding property; or
3. If it will create a need for essential municipal services which cannot be reasonably met.

11-11-060: ACCESSORY BUILDINGS AND STRUCTURES:

A. Location: Accessory buildings, except those listed in subsection B of this section:

1. Shall be separated from the main building by a distance in compliance with applicable building codes;
2. Cannot encroach on any recorded easement;
3. Must be located at least fifteen feet (15') from any dwelling on an adjacent lot;
4. Accessory buildings located to the rear or side of the main building shall not occupy more than twenty five percent (25%) of the rear yard or thirty three percent (33%) of the side yard;
5. Accessory buildings shall, without exception, be subordinate in area to the main building.
6. Any eave, or part of an Accessory building, shall not overhang or extend past a property line.
7. An accessory building may be located in a side corner yard or front yard of a lot; providing, that the building is an architectural and integral part of the main building and in no event shall the accessory building encroach into the required front yard or required side corner yard beyond the nearest corner of the main building.

11-11-070: BUILDING HEIGHT:

A. Main Buildings:

1. Main buildings shall not exceed twenty seven feet (27') in height;
2. No dwelling or structure shall contain less than one story.

B. Accessory Buildings Or Structures (which does not include fences):

1. Accessory buildings or structures shall not exceed fifteen feet (15') in height unless an increased height is approved by the planning commission after review of a conditional use application filed by the property owner. No fee shall be assessed for such application.
2. Accessory buildings within one foot (1') of a side property line located in the side yard or front yard shall be limited to ten feet (10') in height and an increase in height of one (1') may be allowed for each additional foot setback from the side property, but not to exceed the maximum height for such buildings unless as otherwise provided herein.
3. Accessory buildings shall, without exception, be subordinate in height to the main building. (Ord. 2005-11, 4-6-2005; amd. Ord. 2019-23, 7-16- 2019)

LOT# 604  
 SPRING CREEK ESTATES  
 1714 WEST SPRING MEADOW LANE  
 FARMINGTON, UTAH, 84025



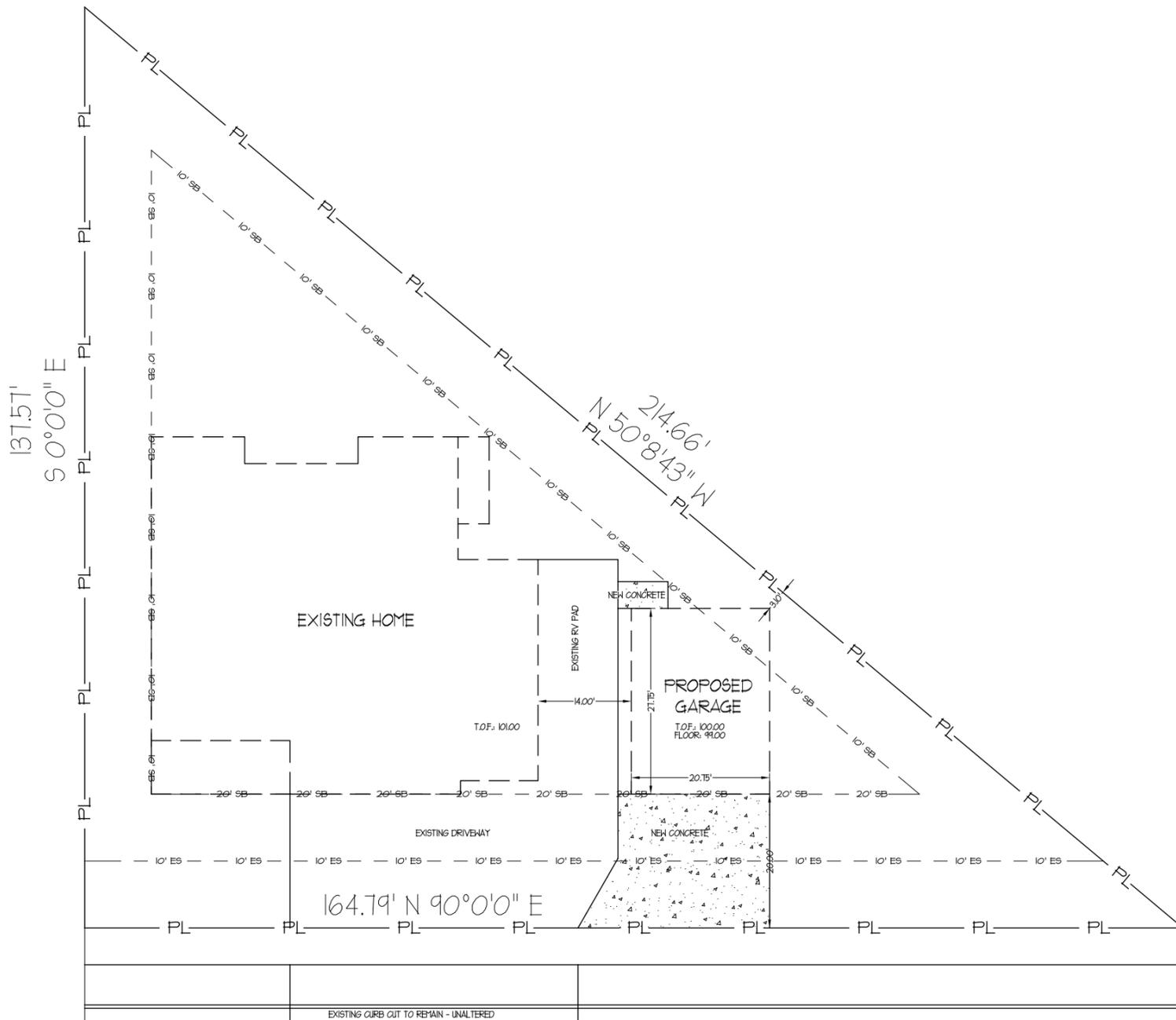
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 THIS PLAN IS PROPERTY OF SKY'S THE LIMIT DESIGN AND SHALL NOT BE REPLICATED IN ANY FORM OR USED FOR THE BASIS OF ANY NEW PLANS  
 THE BUILDER/GENERAL CONTRACTOR (CONSTRUCTION PROFESSIONAL) MUST CAREFULLY AND THOROUGHLY VERIFY DIMENSIONS, VALIDITY, AND OVERALL INTEGRITY OF THE PLANS IN THE EVENT OF A DISCREPANCY, PRIOR TO CONSTRUCTION. SKY'S THE LIMIT DESIGN SHALL BE CONTACTED FOR CLARIFICATION.

- ALL GRADES SHOWN ARE A MINIMUM GRADE FOR THE REQUIRED DRAINAGE TO THE STREET WITH THE REQUIRED 5% SLOPE FOR THE FIRST 10' FROM THE FOUNDATION
1. THE GRADE AWAY FROM THE FOUNDATION WALLS SHALL FALL A MINIMUM OF 6" WITHIN THE FIRST 10' (5%)
  2. TOP OF FOUNDATION MUST BE SET HIGH ENOUGH TO ALLOW 6" EXPOSED, 6" FALL IN 10' (5% SLOPE FOR 10'), AND ENOUGH DROP TO ALLOW DRAINAGE OF LOT IN COMPLIANCE WITH GRADING AND SITE PLANS.
  3. ALL STORM WATER AND DIRT WILL BE KEPT ONSITE DURING CONSTRUCTION UNTIL FINAL LANDSCAPING IS DONE. GENERAL CONTRACTOR WILL BE HELD RESPONSIBLE FOR KEEPING DIRT/MUD ONSITE DURING BAD
  4. WEATHER AND FOR CLEANING UP AFTER SUBCONTRACTORS.
  5. STREET, CURB AND GUTTER WILL BE INSPECTED AND CLEANED OF ALL MUD AND DIRT AT THE END OF EVERY DAY.
  6. GRAVEL BAGS TO BE PLACED AND MAINTAINED AROUND ANY STORM DRAIN INLET ADJACENT TO OR
  7. IMMEDIATELY DOWN STREAM FROM SITE DURING CONSTRUCTION.
  8. ALL LOT CORNER SURVEY MARKERS TO BE LOCATED (OR RE-SET TO ALLOW FOOTING SETBACKS TO BE CHECKED.
  9. BERRIS OR SHALES MAY BE REQUIRED ALONG PROPERTY LINES TO PREVENT STORM WATER FLOW ONTO ADJACENT LOTS. FINAL GRADING SHALL BLEND WITH ADJACENT LOTS.
  10. A LINED CONCRETE WASHOUT AREA MUST BE PROVIDED AT THE SITE FOR ALL CONCRETE, PAINT, STUCCO, OR MASONRY WORK. WASHOUT ONTO THE GROUND IS PROHIBITED.
  11. FINISHED GRADE MUST DIRECT BACKYARD DRAINAGE TO INLET BOXES. GRADE MUST MATCH ELEVATION AT INLET TO ALLOW WATER TO ENTER BOXES.
  12. DRAINAGE FROM THE PROPERTY TO THE SOUTH MUST BE ALLOWED TO ENTER THE LOT 4 DRAIN TO INLET BOXES AFTER LANDSCAPING HAS BEEN COMPLETED/INSTALLED

- LINETYPE KEY**
- PL ——— PROPERTY LINE
  - 10' SB — 10' SETBACK
  - 20' SB — 20' SETBACK
  - 10' ES — 10' EASEMENT
  - - - - - TOPOGRAPHY LINE

SITE PLAN ( DETACHED OPT )

SCALE: 12X18: 1" = 20' | 24X36: 1" = 10'



PROJECT DETAILS:  
**NIELSEN GARAGE**  
 LOT # 604 SPRING CREEK ESTATES  
 1714 WEST SPRING MEADOW LANE  
 FARMINGTON, UTAH, 84025

**SKY'S THE LIMIT DESIGN**  
 (385) 626-5568  
 LAYTON CITY, UTAH

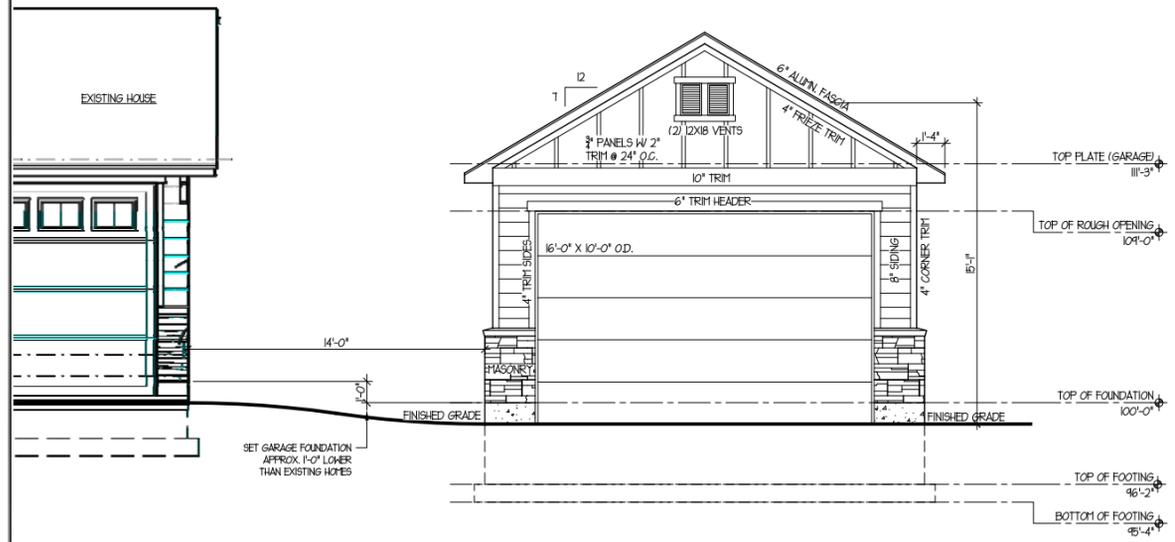
START DATE:  
 MAY. 13, 2020

PLAN NAME:  
 NIELSEN GARAGE

PLAN NUMBER:  
 G-540-20

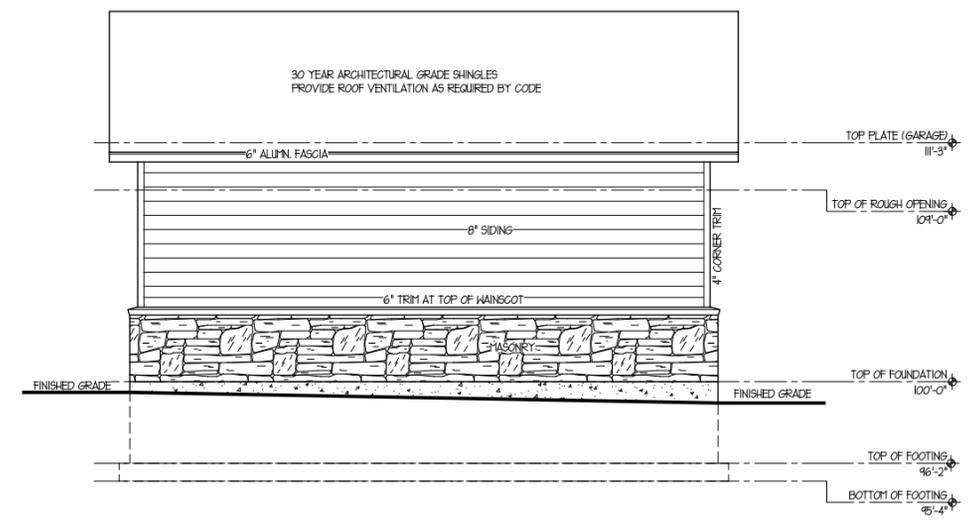
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 THIS PLAN IS PROPERTY OF SKY'S THE LIMIT DESIGN AND SHALL NOT BE REPLICATED IN ANY FORM OR USED FOR THE BASIS OF ANY NEW PLANS  
 THE BUILDER/GENERAL CONTRACTOR (CONSTRUCTION PROFESSIONAL) MUST CAREFULLY AND THOROUGHLY VERIFY DIMENSIONS, VALIDITY, AND OVERALL INTEGRITY OF THE PLANS IN THE EVENT OF A DISCREPANCY, PRIOR TO CONSTRUCTION. SKY'S THE LIMIT DESIGN SHALL BE CONTACTED FOR CLARIFICATION.



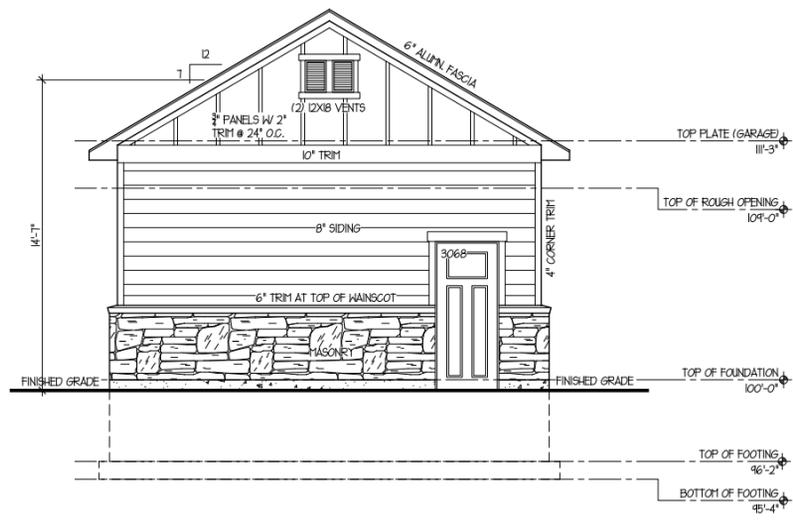
**FRONT ELEVATION**

SCALE: 12X18: 1/8" = 1'-0" | 24X36: 1/4" = 1'-0"



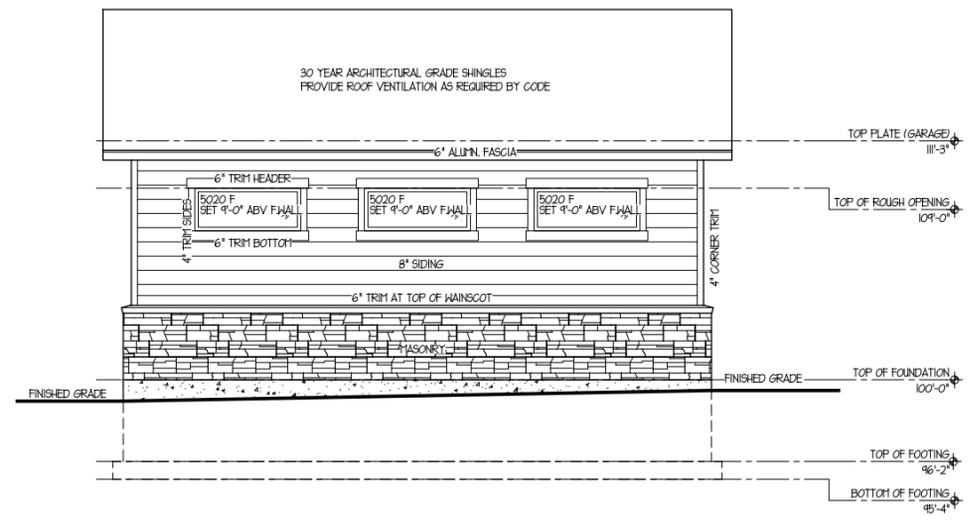
MATERIALS AND CALLOUTS SAME AS FRONT ELEVATION UNLESS OTHERWISE SPECIFIED  
**LEFT ELEVATION**

SCALE: 12X18: 1/8" = 1'-0" | 24X36: 1/4" = 1'-0"



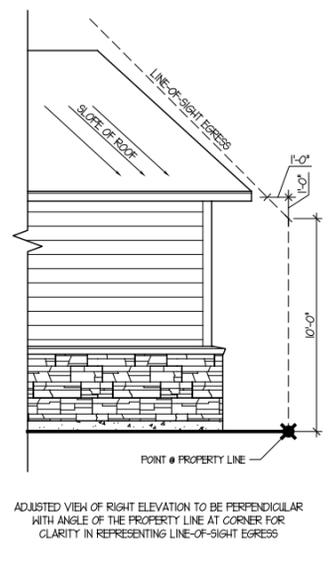
MATERIALS AND CALLOUTS SAME AS FRONT ELEVATION UNLESS OTHERWISE SPECIFIED  
**REAR ELEVATION**

SCALE: 12X18: 1/8" = 1'-0" | 24X36: 1/4" = 1'-0"



MATERIALS AND CALLOUTS SAME AS FRONT ELEVATION UNLESS OTHERWISE SPECIFIED  
**RIGHT ELEVATION**

SCALE: 12X18: 1/8" = 1'-0" | 24X36: 1/4" = 1'-0"



ADJUSTED VIEW OF RIGHT ELEVATION TO BE PERPENDICULAR WITH ANGLE OF THE PROPERTY LINE AT CORNER FOR CLARITY IN REPRESENTING LINE-OF-SIGHT EGRESS

PROJECT DETAILS:  
**NIELSEN GARAGE**  
 LOT # 604 SPRING CREEK ESTATES  
 1774 WEST SPRING MEADOW LANE  
 FARMINGTON, UTAH, 84025

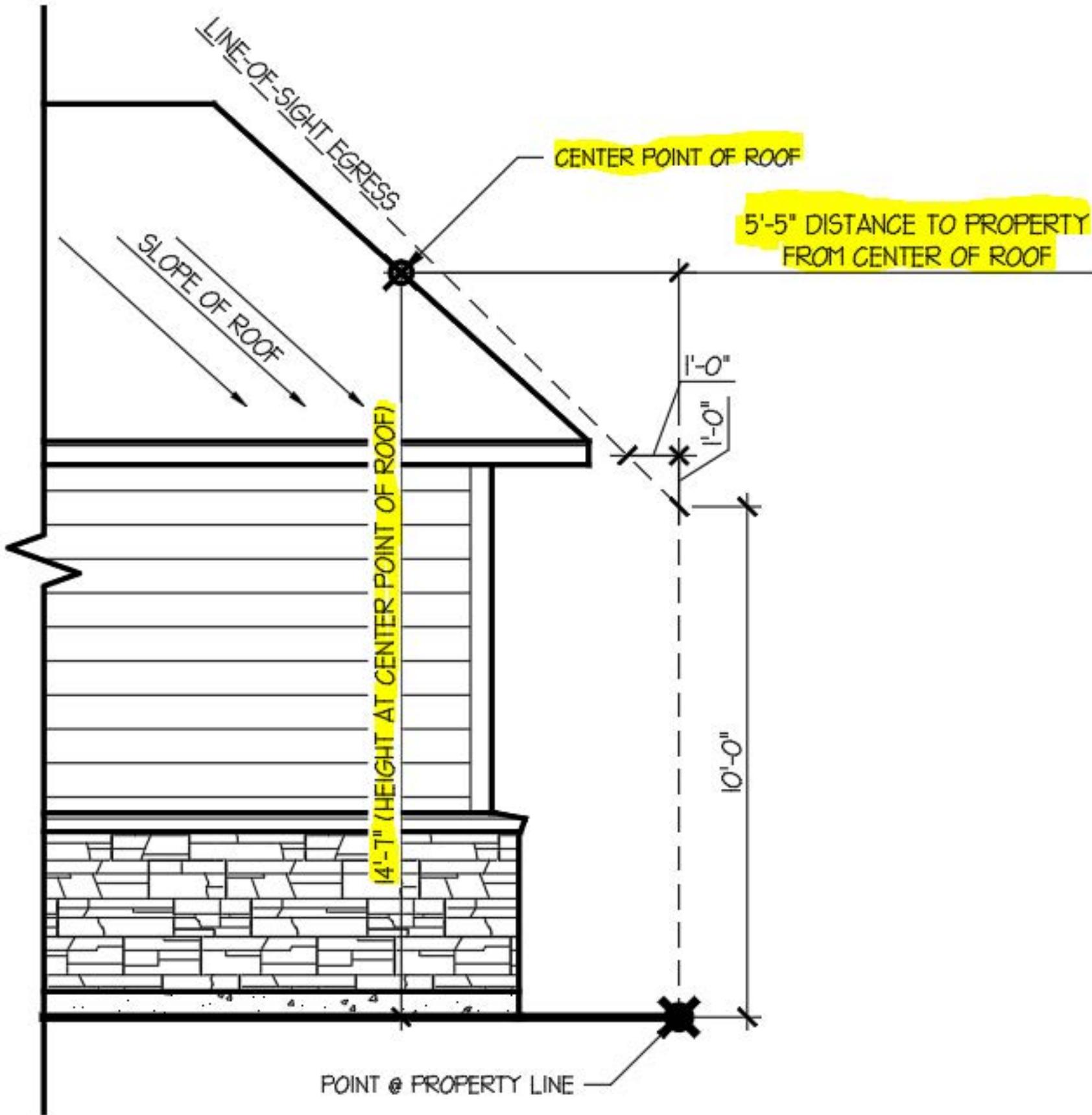
**SKY'S THE LIMIT DESIGN**  
 (385) 626-5568  
 LAYTON CITY, UTAH

START DATE:  
 MAY 13, 2020

PLAN NAME:  
 NIELSEN GARAGE

SHEET: A3.0

PLAN NUMBER:  
 G-540-20





## Planning Commission Staff Report July 9, 2020

---

### Item 7: Conditional Use Permit and Site Plan Approval for Farmington Bay Storage Phase 3

Public Hearing: Yes  
Application No.: C-11-19/SP-5-19  
Property Address: 1272 S. 650 W.  
General Plan Designation: LM (Light Manufacturing)  
Zoning Designation: LM&B (Light Manufacturing and Business)  
Area: 2.82  
Number of Lots: 1  
Property Owner: Bradley D. Pack – Trustee  
Agent: JD Tyrell/Wasatch West Contracting

Request: *Conditional Use and Site plan approval for the Farmington Bay Phase 3 Self-Storage Facility.*

---

#### **Background Information**

An application has been submitted for conditional use and site plan approval for the Farmington Bay Storage Phase 3 self-storage facility. The applicant is proposing a facility that has 38,100 square feet footprint on of 2.82 acres of property. Section 11-26-040 lists mini-warehousing/self-storage as a Conditional Use, therefore, requiring review by the Planning Commission. This application has met the development standards outlined in Section 11-26-080 to staff's satisfaction including landscaping and buffering requirements for self-storage. Staff requests all further review of the project be dedicated to staff.

#### **Suggested Motion**

Move the Planning Commission approve the conditional use and site plan subject to all applicable Farmington City ordinances and development standards and the following conditions:

1. Lighting shall be designed, located and directed in a way so as to eliminate glare and minimize reflection of light to neighboring properties.
2. The hours of operation be limited to 8 am to 10 pm.
3. Any signs proposed for the project must comply with the Farmington City Sign Ordinance.
4. The applicant must obtain all other applicable permits for the operation of the conditional use including but not limited to a business license from Farmington City, all health department regulations and meet all applicable building and fire codes.
5. The applicant must address all outstanding DRC Comments.
6. The final site plan shall be reviewed and approved by staff.

7. This approval enables the applicant to move forward with a one lot platted subdivision so long as it meets all standards of the DRC.

#### Findings for Approval

1. The proposed use of the particular location is necessary and desirable and provides a service that contributes to the general well-being of the community.
2. The proposed use complies with all regulations and conditions in the Farmington City Zoning Ordinance for this particular use.
3. The proposed use conforms to the goals, policies, and principles of the Comprehensive General Plan.
4. The proposed use is compatible with the character of the site and adjacent properties,
5. The location provides or will provide adequate utilities, transportation access, drainage, parking and loading space, lighting, screening, landscaping and open space, fire protection, and safe and convenient pedestrian and vehicular circulation.
6. The proposed use is not detrimental to the health, safety, and general welfare of persons residing or working in the vicinity.
7. The proposed use provides adequate parking with 34 stalls on site.

#### Supplemental Information

1. Vicinity Map
2. Site Plan
3. Elevations
4. Landscape Plan

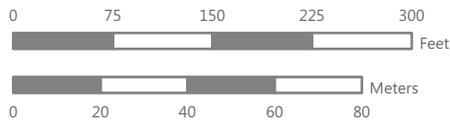
#### Applicable Ordinances

1. Title 11, Chapter 7 – Site Development
2. Title 11, Chapter 8 – Conditional Uses
3. Title 11, Chapter 26 – Light Manufacturing and Business (LM&B)
4. Title 11, Chapter 28 – Supplementary and Qualifying Regulations



# VICINITY MAP

## Farmington Bay Storage



Disclaimer: This map was produced by Farmington City GIS and is for reference only. The information contained on this map is believed to be accurate and suitable for limited uses. Farmington City makes no warranty as to the accuracy of the information contained for any other purposes.



**AREA TABULATION**

	sq.ft.	Acres	%
BUILDING	38,100	0.87	31.03%
IMPROVEMENTS	69,160	1.59	56.32%
LANDSCAPE	15,537	0.35	12.65%
<b>TOTAL</b>	<b>122,797</b>	<b>2.82</b>	<b>100.00%</b>

**PARKING CALCULATIONS**

	OFFICE
BUILDING SQ. FT. (GROSS)	38,100
MANUFACTURING/INDUSTRIAL PARKING RATIO	1:1000 SQ. FT.
REQUIRED PARKING	32
REQUIRED ADA PARKING	2
<b>TOTAL DESIGNED PARKING</b>	<b>34</b>
<b>TOTAL ADA PARKING</b>	<b>2</b>

**SYMBOL LEGEND**

- (C-1) ASPHALT & BASE PER DETAIL, SHEET DT100
  - (C-2) 36" CONCRETE WATERWAY PER DETAIL, SHEET DT100
  - (C-3) CONCRETE DRIVE APPROACH PER DETAIL, SHEET DT100
  - (R-1) MINOR RETAINING WALL PER DETAIL, SHEET DT100
  - (A-1) ADA SIDEWALK RAMP AND PARKING AREA PER DETAIL, SHEET DT100
  - (A-2) ADA VAN ACCESSIBLE PARKING SIGN PER DETAIL, SHEET DT100
  - (P-1) 30" CONCRETE CURB AND GUTTER REPLACEMENT PER FARMINGTON CITY STANDARDS
  - (P-2) SIDEWALK IS 6" THICK WITH FIBER, WITH 4" OF BASE COURSE, PER FARMINGTON CITY STANDARDS
- ALL ITEMS WITHIN THE PUBLIC RIGHT-OF-WAY TO CONFORM TO THE RIGHT-OF-WAY OWNER'S STANDARDS & SPECIFICATIONS.

**ACCESSIBLE AREA CONSTRAINTS**

ALL ACCESSIBLE AREAS ARE TO MAINTAIN THE FOLLOWING MAXIMUM SLOPES AND TOLERANCES:

**ACCESSIBLE PARKING:**  
MAXIMUM SLOPE OF 1:48 (2%) THROUGHOUT.

**ACCESSIBLE ROUTE:**  
MINIMUM WIDTH OF 48". MAXIMUM SLOPE OF 1:20 (5%) ALONG THE ROUTE, MAXIMUM CROSS-SLOPE OF 1:48 (2%).

**ACCESS ROUTE TURNAROUNDS:**  
A CLEAR 60" TURNING DIAMETER, MAXIMUM SLOPE OF 1:48 (2%) IN ANY DIRECTION.

**LEVEL LANDING / EXTERIOR DOOR LANDING:**  
MINIMUM SIZE OF 60"x60". MAXIMUM SLOPE OF 1:48 (2%) IN ANY DIRECTION.

**ACCESSIBLE EGRESS TO PUBLIC WAY:**  
MAXIMUM SLOPE OF 1:20 (5%) ALONG THE ROUTE, MAXIMUM CROSS-SLOPE OF 1:48 (2%).

**ADA ACCESS RAMPS:**  
MAXIMUM SLOPE OF 1:12 (8.33%), WITH A MAXIMUM CROSS-SLOPE OF 2%. THE TRANSITION BETWEEN ASPHALT AND CONCRETE IS NOT TO EXCEED 1/2" VERTICAL (1/4" IF BEVELED).

**CITY ENGINEER'S APPROVAL**

APPROVED BY THE FARMINGTON CITY ENGINEER, THIS \_\_\_\_\_ DAY OF \_\_\_\_\_, 20\_\_\_\_.

FARMINGTON CITY ENGINEER

1470 South 600 West  
Woods Cross, UT 84010  
Phone 801.298.2236  
www.Entellus.com



Client  
Contact  
Phone #  
Address  
Email

**FARMINGTON BAY STORAGE**

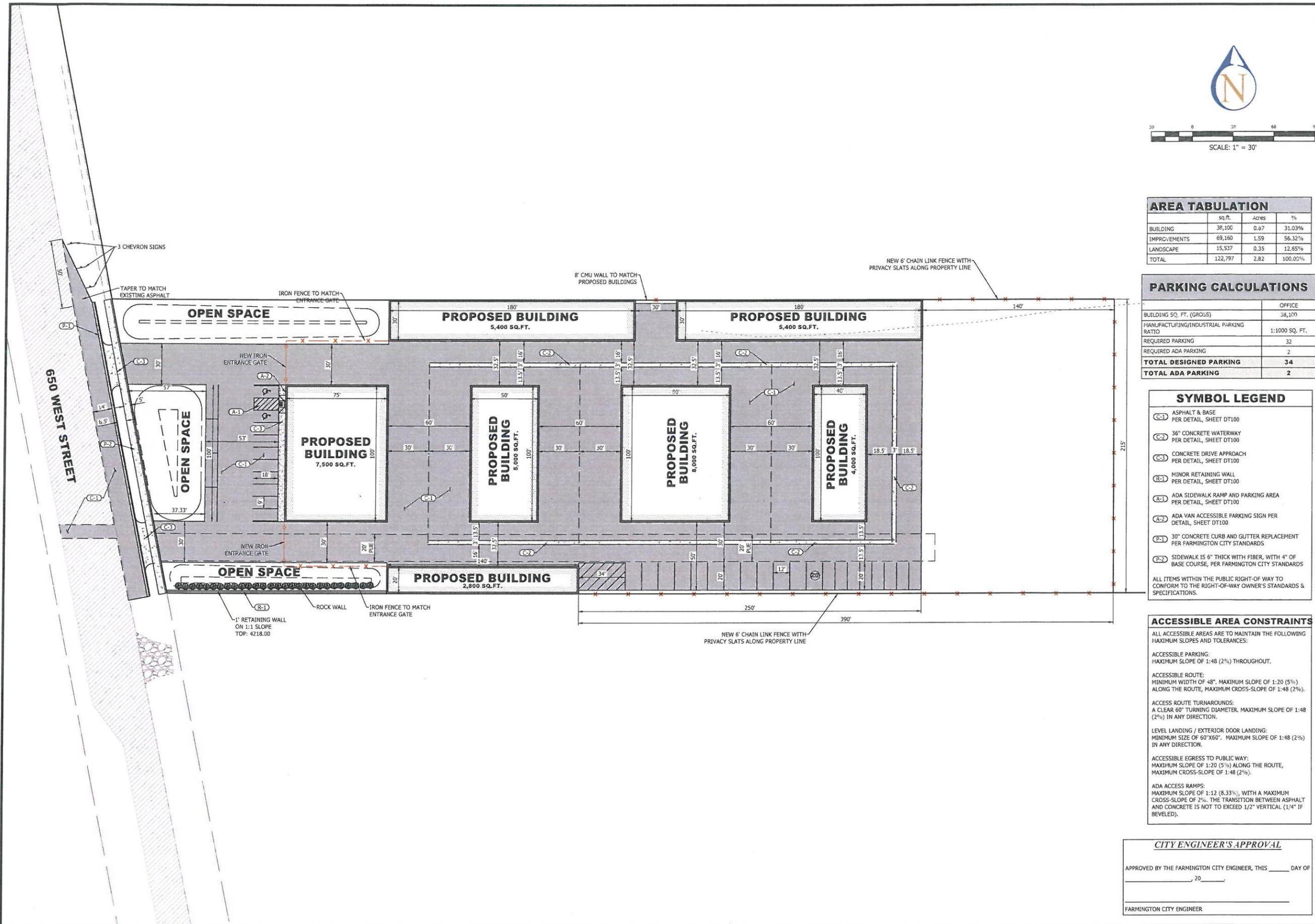
650 WEST STREET

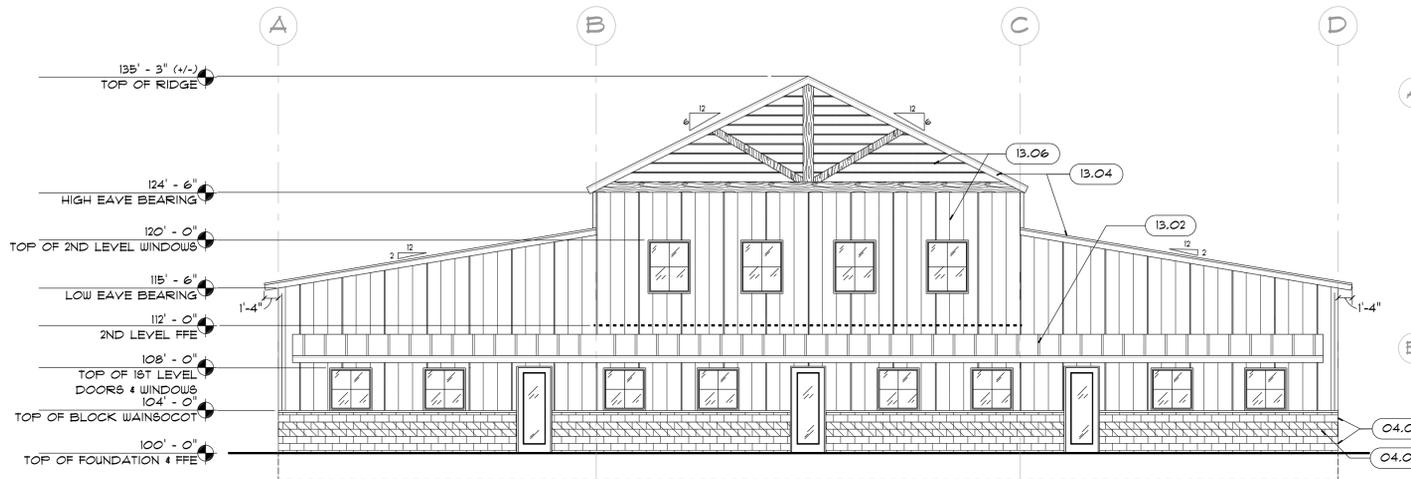
LOCATED IN THE SW 1/4 OF SECTION 31, T.4., R.3., S.L.H.&M.  
FARMINGTON CITY, DAVIS COUNTY, UTAH

REV#	DATE	REVISION

DRAWN: R/NH 6/3/20  
 ENGINEER: STA 6/3/20  
 PROJECT #: 1053004  
 PLAN SET-1053004.dwg

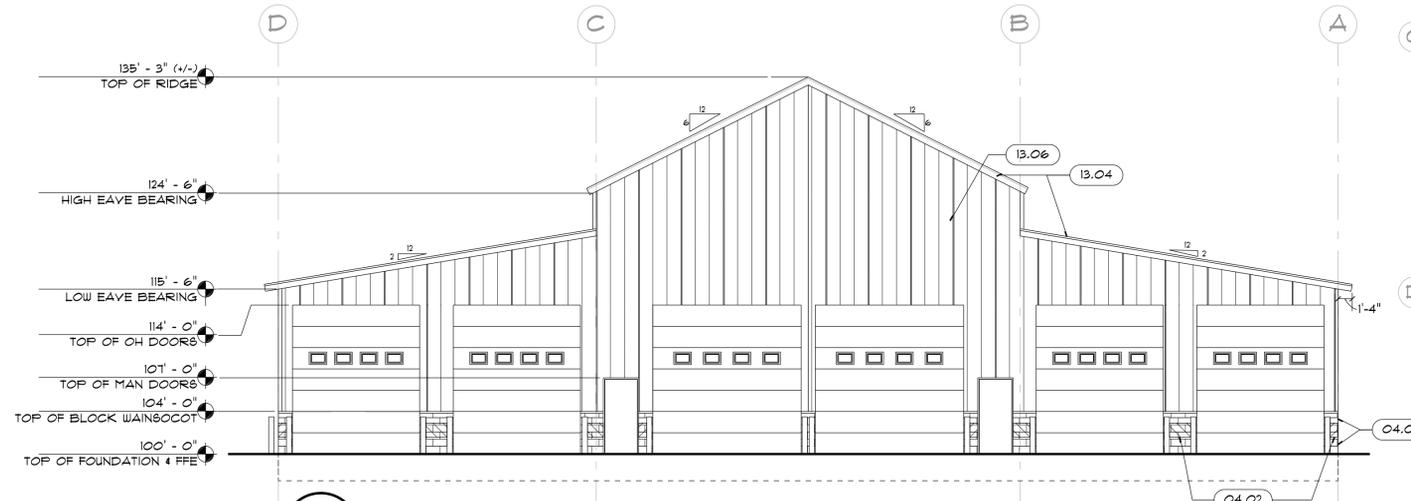
**C300**  
SITE PLAN



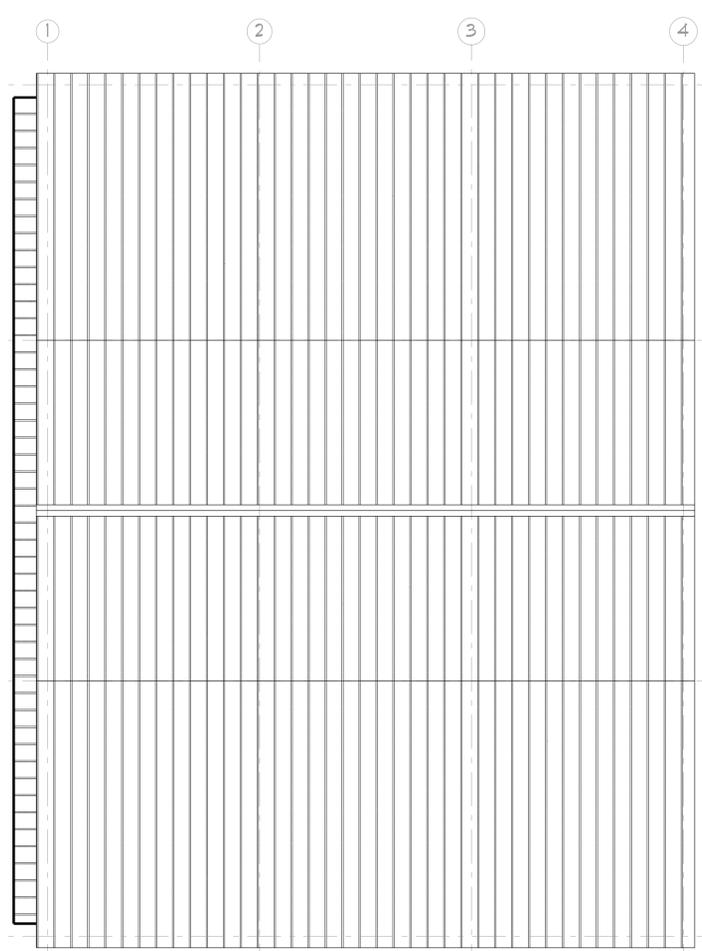


**D1 WEST ELEVATION (BLDG. C)**  
A302 | SCALE: 1/8" = 1'-0"

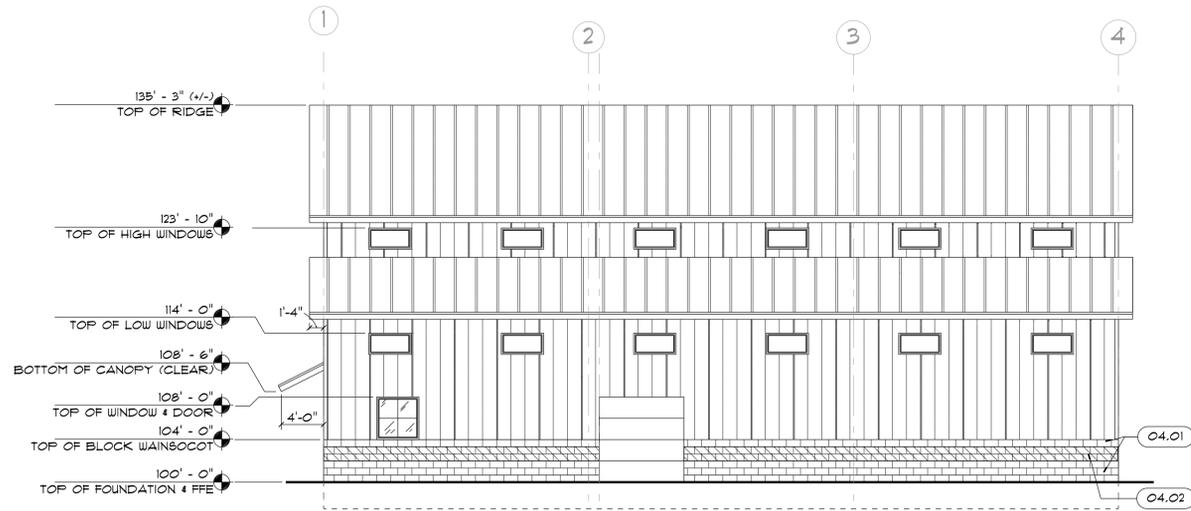
ALL PEMB COLUMNS TO SIT AT ELEVATION 100'-0"  
BASE ANGLE & TRIM SET AT 104'-0"



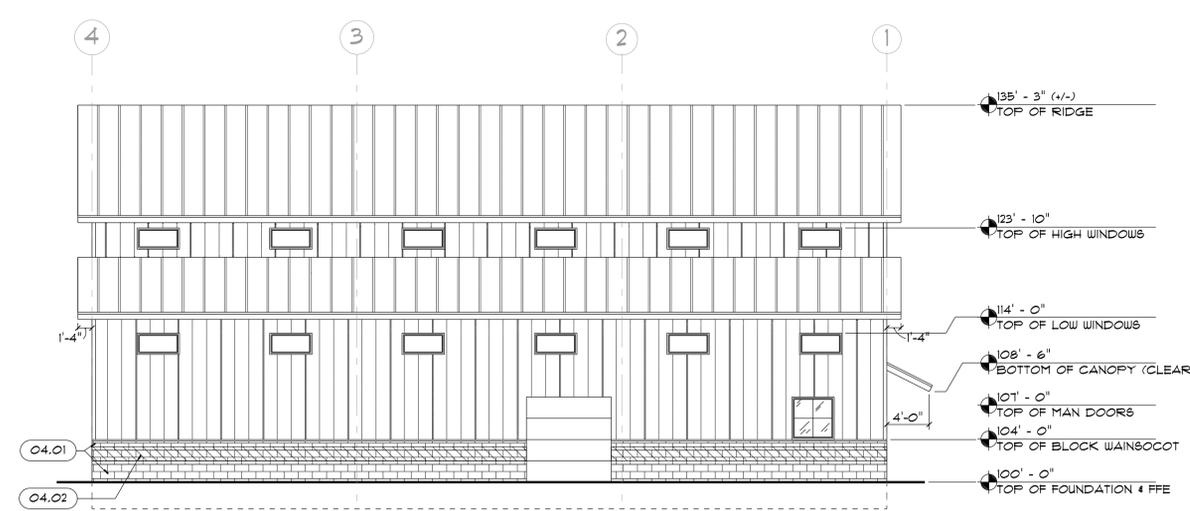
**D2 EAST ELEVATION (BLDG. C)**  
A302 | SCALE: 1/8" = 1'-0"



**D5 ROOF PLAN (BLDG. C)**  
A301 | SCALE: 1" = 10'



**D3 SOUTH ELEVATION (BLDG. C)**  
A302 | SCALE: 1/8" = 1'-0"



**D4 NORTH ELEVATION (BLDG. C)**  
A302 | SCALE: 1/8" = 1'-0"

**KEY NOTES**

<b>03 CONCRETE</b>	03.01 4" INTERIOR SLAB ON GRADE OVER 4" GRAVEL BASE (SEE STRUCTURAL)
	03.02 FOOTING & REINFORCEMENT (SEE STRUCTURAL)
<b>04 MASONRY</b>	04.01 COLORED SMOOTH CMU (COLOR BY OWNER)
	04.02 2 COURSE COLORED SPLIT FACE CMU BAND (COLOR BY OWNER)
	04.03 SINGLE COURSE COLORED, SMOOTH, CMU BAND (COLOR BY OWNER)
<b>05 METALS</b>	05.01 STEEL EMBED FOR ROOF CONNECTION (SEE STRUCTURAL FOR SIZE AND LOCATION)
<b>07 THERMAL &amp; MOISTURE</b>	07.01 R-19 FIBERGLASS BATT INSULATION W/WMPYR FACING
<b>08 OPENINGS</b>	08.01 COILING OR OVERHEAD DOOR (SEE DOOR SCHEDULE)
	08.02 FILLER PANEL ABOVE DOOR TO MATCH DOOR (DPCI OR EQUAL)
	08.03 SECTIONAL OVERHEAD DOOR (SEE DOOR SCHEDULE)
<b>11 EQUIPMENT</b>	11.01 ENTRY KEYPAD
	11.02 FUTURE KIOSK (BY OWNER)
<b>13 SPECIAL CONSTRUCTION</b>	13.01 INTERIOR WALL FRAMING & SHEETING (BY PEMB SUPPLIER)
	13.02 STANDING BEAM METAL ROOF, CAP, & ROOF STRUCTURE (BY PEMB SUPPLIER)
	13.03 NO SNOW GUTTER (BY PEMB SUPPLIER)
	13.04 GABLE, HIGH & LOW EAVE FLASHING (BY PEMB SUPPLIER)
	13.05 DOWN SPOUT (BY PEMB SUPPLIER)
	13.06 METAL SIDING, TRIM, AND BASE ANGLE TRIM (BY PEMB SUPPLIER)
<b>22 PLUMBING</b>	22.01 HOT WATER HEATER (SEE PLUMBING PLAN)
<b>23 HVAC</b>	23.01 FURNACE (SEE MECHANICAL PLAN)
<b>32 EXTERIOR IMPROVEMENTS</b>	32.01 4" STEEL BOLLARD W/24"x36" DEEP FOOTING TYPICAL @ ALL BUILDING CORNERS
	32.02 ASPHALT PAVING & UBC (PER CIVIL DRAWINGS)
	32.03 IMPORTED OR NATIVE COILS (PER SOILS REPORT)



**WASATCH WEST CONTRACTING, LLC**  
PO BOX 160442  
CLEARFIELD, UT 84016  
P:801.820.5320  
F:801.823.2242  
www.wasatchwestllc.com

**FARMINGTON BAY STORAGE PH. 3**

ADDRESS TBD.  
FARMINGTON, UT 84125

REVISIONS  
DRAWN BY: JDT  
CHECKED BY:  
ISSUE DATE: 5/30/20  
WWC PROJECT NO.: 2333  
SHEET NAME: **BLDG C ELEVATIONS & SECTIONS**  
SHEET NO.: **A302**



**P1** EXISTING STORAGE OFFICE  
A700 | N.T.S.



**P2** ENTRY GATE  
A700 | N.T.S.



**P3** STORAGE BUILDING BORDERING STREET  
A700 | N.T.S.



**P4** MAIN ENTRANCE DRIVE (LOOKING NORTH)  
A700 | N.T.S.



**P5** MAIN ENTRANCE DRIVE (LOOKING SOUTH)  
A700 | N.T.S.



**P6** CHAIN LINK FENCE W/PRIVACY SLATS  
A700 | N.T.S.

**KEY NOTES**

- 1 STORAGE OFFICE WITH STONE WAINSCOTT
- 2 STEEL FRONT ENTRY GATE & FENCE
- 3 COLORED CMU WALLS AT STORAGE BUILDINGS
- 4 COLORED SPLIT FACE CMU ACCENT BAND
- 5 COLORED ACCENT BAND AT TOP OF WALL
- 6 COILING DOORS FOR UNIT ACCESS
- 7 EXAMPLE OF PROPOSED CHAIN LINK FENCE WITH PRIVACY SLATS  
COLOR OF SLATS AS RECOMMENDED BY FARMINGTON CITY



**WASATCH WEST CONTRACTING, LLC**  
PO BOX 160442  
CLEARFIELD, UT 84016  
P.801.820.5320  
F.801.823.2242  
www.wasatchwestllc.com

ARCHITECT / ENGINEERS STAMP

**FARMINGTON BAY STORAGE PH. 3**

ADDRESS TBD.  
FARMINGTON, UT 84125

REVISIONS

DRAWN BY: JDT  
CHECKED BY:  
ISSUE DATE: 5/30/20  
WWC PROJECT NO.: 2333  
SHEET NAME  
**EXISTING EXTERIOR FINISHES**

SHEET NO.  
**A700**



## Planning Commission Staff Report July 9, 2020

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### Item 8A: Saltzgeber Special Exception for Moderate Income Dwelling Unit

Public Hearing:	Yes
Application No.:	M-5-20
Property Address:	1454 South 200 East
General Plan Designation:	LDR
Zoning Designation:	R-2
Area:	0.21 acres
Number of Lots:	1
Applicant:	JMSRE Investments LLC (John Saltzgeber, Property Owner)

*Request: Approval of a special exception allowing one moderate-income dwelling unit*

---

### **Background Information**

In November 2019, the City Council approved an amendment to the Zoning Ordinance 11-03-045 defined a special exception to include, “additional dwelling units to provide moderate-income households”. The applicant is requesting that a moderate-income housing unit be allowed on his property at 1454 South 200 East, Lot 2 of Aegean Village Subdivision Plat “A”.

Currently, an old garage home sits on the property, with zoning allowing up to a duplex. Previously, the property was zoned R, but the City Council decided to rezone to R-2 on July 16, 2019. The property bordered an R-2 zone and it was found that the R-2 zone would support the General Plan designation of LDR. Instead of two dwelling units within a duplex (two-family dwelling), the owner desires to create two for-sale townhomes. As well as one for-sale moderate income housing unit.

Now the applicant is in the process of attempting to create three lots on this parcel. The purpose would be to create three townhomes, two of which would be for sale at market-rate, with the third requiring a special exception to be built and sold as moderate-income. In order to achieve the City Council must also approve a plat amendment. Approval of this unit would support Farmington’s recently-adopted moderate-income housing plan, while increasing density for the owner and the City. This unit, like any special exception, will be subject to reviews of location, design, and other impacts.

### **Suggested Motion**

Move that the Planning Commission approve the special exception request subject to all applicable Farmington City ordinances and standards, and the following conditions:

1. The property will be subject to UDOT approval, if necessary.
2. The applicant shall prepare a landscaping plan for review and approval by City staff.
3. The applicant must construct sufficient screening (as reviewed and approved by City staff), along the west and north boundaries of the property to protect against light and noise nuisances to surrounding properties.
4. The owner shall enter into an agreement the City Council to establish a moderate income housing unit which must include, but not be limited to the following terms:
  - a. Principal, interest, taxes, and insurance (PITI) cannot be more than 30%
  - b. A tiered approach to sale, with each subset prioritized for 60 days, in the following order:
    - i. Public employees who work in Farmington who make up to 120% of Area Median Income (AMI)
    - ii. Any person who works in Farmington up to 100% of AMI
    - iii. Any person who meets the AMI
    - iv. Open to market
  - c. Terms must be adhered to for 15 years
  - d. Restrict subsequent sales to up to 3% of original sale value per year until 15 year restriction expires
  - e. Enforcement provisions:
    - i. Deed restriction
    - ii. Annual verification
5. A plat amendment must be approved to create three new lots from the existing one.
6. Any comments from the DRC will be addressed prior to recordation.

### **Findings:**

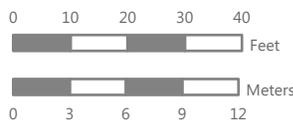
1. The amendment fulfills a strategy from Farmington's moderate income housing plan, in agreement with the General Plan.
2. The amendment creates rental flexibility in Farmington, providing moderate income housing options to more citizens using a tiered approach.
3. The addition of a moderate-income unit will not be detrimental to the health, safety, or general welfare of persons residing in the vicinity, or injurious to property or improvements.
4. Because the site is already zoned to allow a duplex, and as such, allows for two families to contribute to traffic, it is reasonable to assume that the addition of a third unit would not create unreasonable traffic hazards.
5. Similarly, as the lot is already zoned to accommodate a duplex, the parcel will fit an additional unit.

Supplementary Information:

1. Vicinity Map
2. Application including petition and conceptual drawings
3. Section 11-3-045



**VICINITY MAP**  
**1454 S 200 E**



Disclaimer: This map was produced by Farmington City GIS and is for reference only. The information contained on this map is believed to be accurate and suitable for limited uses. Farmington City makes no warranty as to the accuracy of the information contained for any other purposes.



M-5-20

Application No.  
For City Use Only

SPECIAL EXCEPTION APPLICATION  
FEE: \$125.00

Date of application: 6-23-2020

Application for property located at: 1454 SOUTH 200 EAST FARMINGTON Zone: \_\_\_\_\_

Property Owner: JMSRE INVESTMENTS LLC (JOHN SALTER) Phone No.: 801-680-8447  
(Current property owner must sign application)

Mailing Address: 583 EAST PHOENIX CIRCLE BOUNTIFUL UT Zip Code: 84010

Email: jsalter8447@msn.com Fax: N/A Cell: 801-680-8447

Developer/Agent: JOHN SALTER (OWNER) Phone No.: \_\_\_\_\_

Mailing Address: SAME AS ABOVE Zip Code: \_\_\_\_\_

Email: \_\_\_\_\_ Fax: \_\_\_\_\_ Cell: \_\_\_\_\_

LEGAL DESCRIPTION, INCLUDING PARCEL IDENTIFICATION NUMBER, AND SITE PLAN FOR THE SUBJECT PROPERTY MUST ACCOMPANY THIS APPLICATION

1. Describe in detail the proposed special exception for which this application is being submitted. (A separate sheet of information may be included if necessary.)

TO AMEND THE PLAT TO ALLOW 3 INDIVIDUAL PARCELS FOR 3 TOWNHOMES (2) NORMAL TOWNHOMES (MARKET) & (1) MODERATE INCOME HOUSING

2. The applicant must submit a plot plan showing the following: (a) applicant's name; (b) site address; (c) property boundaries and dimensions; (d) layout of existing and proposed buildings, parking, landscaping, and utilities; and (e) adjoining property lines and uses within one hundred (100) feet of the subject property.

3. The applicant shall submit all such other and further information or documentation as the Zoning Administrator may deem necessary for a full and proper consideration and disposition of a particular application.

A *special exception* is an activity or use incidental to or in addition to a principal use permitted in a zoning district or an adjustment to a fixed dimension standard permitted as an exception to the requirements of Title II. A special exception has less potential impact than a conditional use but still requires careful review of such factors as location, design, configuration and/or impacts to determine the desirability of authorizing its establishment on any given site. Refer to Section II-3-045 of the Farmington City Zoning Ordinance for further information.

John Saltzgeber  
JMSRE Investments, LLC  
583 East Pheasant Circle  
Bountiful, Utah 84010  
801-680-8447

Farmington City Community Development Department  
160 S. main St.  
Farmington, Utah 84025

## **RE: Petition to Amend Subdivision Plat**

To whom it may concern,

I am the property owner of 1454 South 200 East Farmington Utah, 84025 Parcel # 07-110-0055.

This letter constitutes a formal petition to amend our subdivision plat Lot 2, AEGEAN VILLAGE SUBDIVISION PLAT "A" 161 East 1470 South Farmington, Utah by changing this lot zoned for a duplex into 3 separate lots to build 3 townhomes (1) moderate income townhome and (2) market rate townhomes.

Thank you for your time and consideration in reviewing this Petition.

Sincerely,

A handwritten signature in black ink, appearing to read "John Saltzgeber". The signature is written in a cursive style with a large initial "J" and "S".

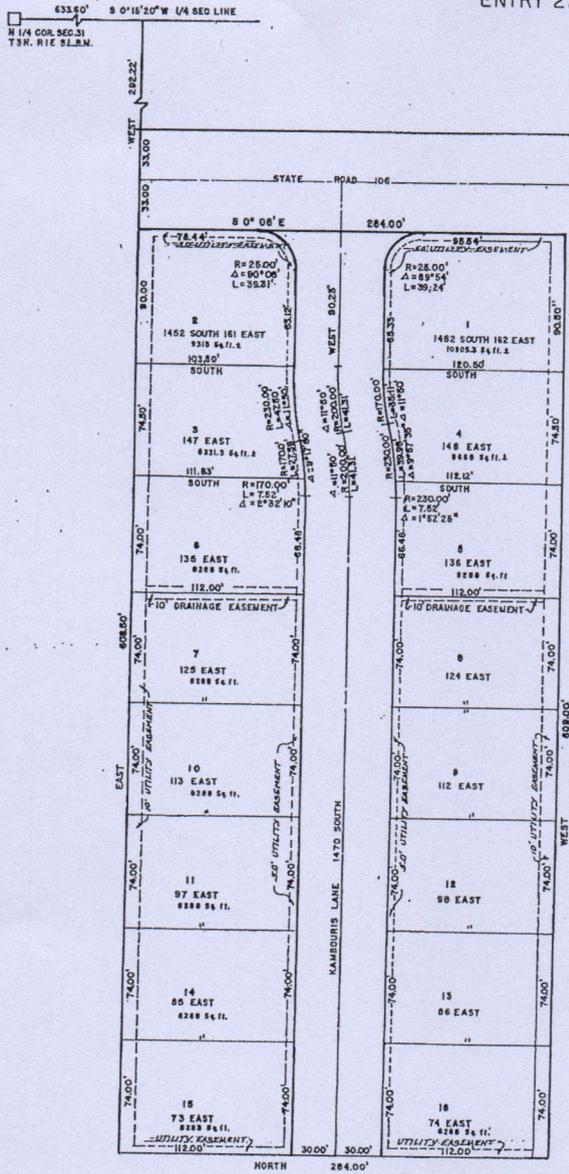
John Saltzgeber  
Owner of JMSRE Investments, LLC

# AEGEAN VILLAGE SUBDIVISION PLAT "A"

A PART OF THE NORTHWEST 1/4 OF SECTION 31, TOWNSHIP 3, NORTH, RANGE 1, EAST S.L.B.N., U.S. SURVEY

FARMINGTON CITY, DAVIS COUNTY, UTAH

SEE ADDRESS AFFIDAVIT 2660-185,  
ENTRY 2756644, 2783897



### BOUNDARY DESCRIPTION

BEGINNING AT A POINT ON THE WEST LINE OF THE STATE ROAD WHICH IS S 0° 15' 20" W 633.60 FEET ALONG THE 1/4 SECTION LINE AND WEST 292.22 FEET FROM THE NORTH 1/4 CORNER OF SECTION 31, TOWNSHIP 3 NORTH, RANGE 1 EAST, SALT LAKE BASE & MERIDIAN RUNNING THENCE S 0° 06' E 284.00 FEET ALONG THE WEST LINE OF THE STATE ROAD, THENCE WEST 609.00 FEET, THENCE NORTH 284.00 FEET, THENCE EAST 608.80 FEET TO THE POINT OF BEGINNING.

### SURVEYOR'S CERTIFICATE

I, MAX B. ELLIOTT, A REGISTERED LAND SURVEYOR HOLDING CERTIFICATE NO. 2760, AS PRESCRIBED UNDER THE LAWS OF THE STATE OF UTAH, DO HEREBY CERTIFY THAT BY THE AUTHORITY OF THE OWNERS, I HAVE MADE A SURVEY OF THE TRACT OF LAND SHOWN ON THIS PLAT AND DESCRIBED HEREWITH AND HAVE SUBDIVIDED SAID TRACT INTO LOTS AND STREETS HEREAFTER TO BE KNOWN AS AEGEAN VILLAGE SUBDIVISION PLAT "A" AND THAT SAME HAS BEEN CORRECTLY SURVEYED AND STAKED ON THE GROUND AS SHOWN.

May 21, 1977  
DATE

Max B. Elliott  
UTAH SURVEYOR NO. 2760



### OWNER'S DEDICATION

KNOW ALL MEN BY THESE PRESENTS THAT WE THE UNDERSIGNED OWNERS OF THE ABOVE DESCRIBED TRACT OF LAND HAVING CAUSED SAME TO BE SUBDIVIDED INTO LOTS AND STREETS TO BE HEREAFTER KNOWN AS AEGEAN VILLAGE SUBDIVISION PLAT "A" DO HEREBY DEDICATE FOR PERPETUAL USE OF THE PUBLIC, ALL PARCELS OF LAND SHOWN ON THIS PLAT FOR PUBLIC USE, AND DO WARRANT AND DEFEND AND SHOWN THE CITY HARMLESS AGAINST ANY EASEMENTS OR OTHER ENCUMBRANCES ON THE DEDICATED STREETS WHICH WILL INTERFERE WITH THE CITY'S USE, MAINTENANCE, AND OPERATION ON THE STREETS.

IN WITNESS WHEREOF WE HAVE SET OUR HAND THIS DAY OF JULY 1977

*[Signatures]*

### ACKNOWLEDGEMENT

ON THE 17th DAY OF June, 1977 THERE PERSONALLY APPEARED BEFORE ME, THE UNDERSIGNED NOTARY PUBLIC, THE SIGNERS OF THE OWNER'S DEDICATION. IN HUNTER WHO ORLY ACKNOWLEDGED TO ME THAT THEY SIGNED IT FREELY AND VOLUNTARILY AND FOR THE USES AND PURPOSES THEREIN MENTIONED.

NOTARY PUBLIC: *[Signature]*  
RESIDENCE: *[Address]* COMMISSION EXPIRES: *[Date]*

### PLANNING COMMISSION APPROVAL

APPROVED THIS 12 DAY OF July 1977 BY THE FARMINGTON CITY PLANNING COMMISSION

CHAIRMAN: *[Signature]*

### FARMINGTON CITY COUNCIL

PRESENTED TO THE CITY COUNCIL OF FARMINGTON, UTAH THIS 21 DAY OF July, 1977 AT WHICH TIME THIS SUBDIVISION WAS APPROVED AND ACCEPTED.

MAYOR: *[Signature]*  
ATTEST: *[Signature]*



### CITY ENGINEER'S APPROVAL

APPROVED THIS 19th DAY OF July 1977.

CITY ENGINEER: *[Signature]*

### DAVIS COUNTY RECORDER

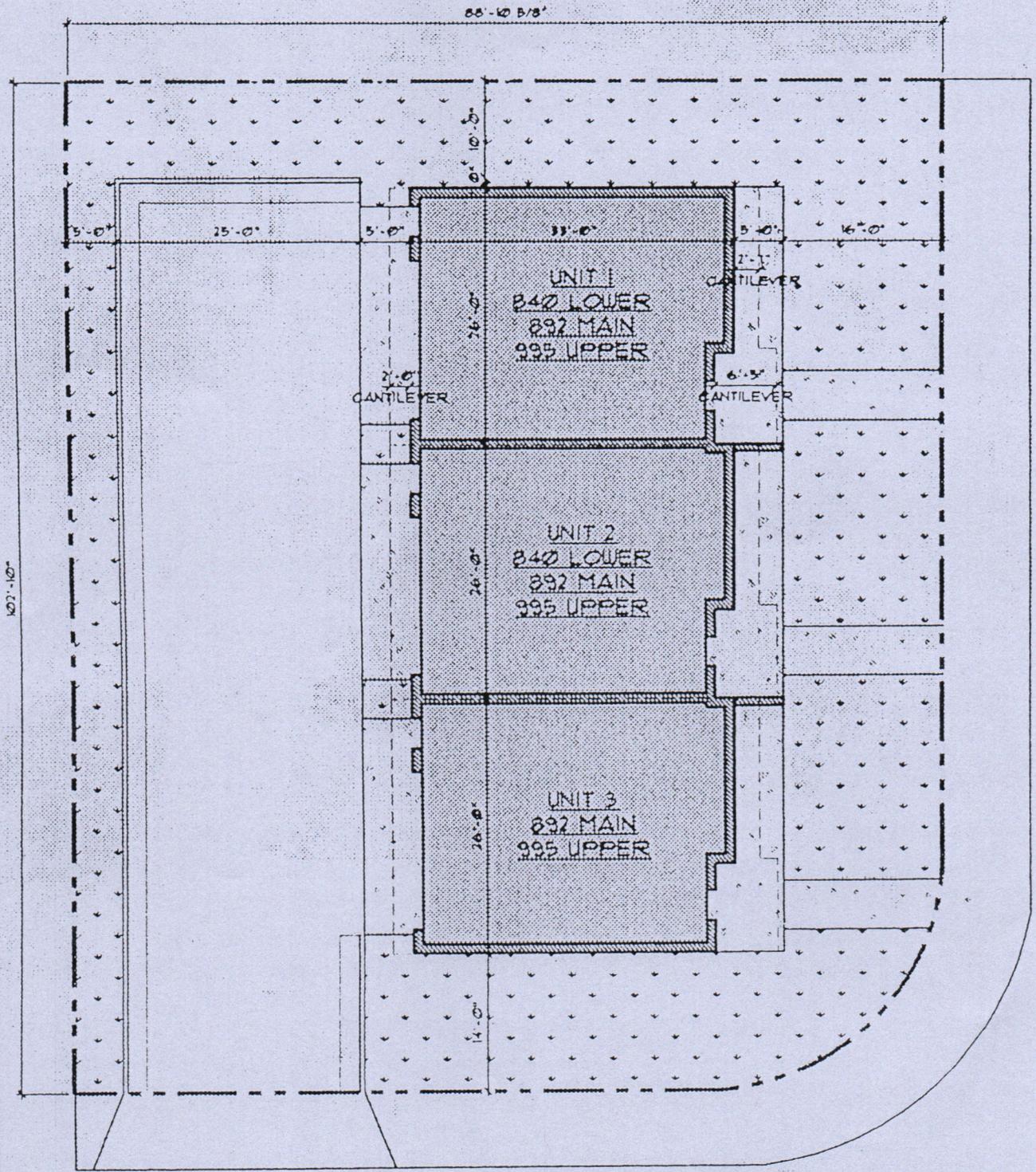
ENTRY NO. 266445 FEE PAID \$8.00 FILED FOR RECORD AND RECORDED THIS 28th DAY OF JULY 1977 AT 2:00 PM IN BOOK 460 OF D. C. - PAGE 830  
COUNTY RECORDER: *[Signature]*

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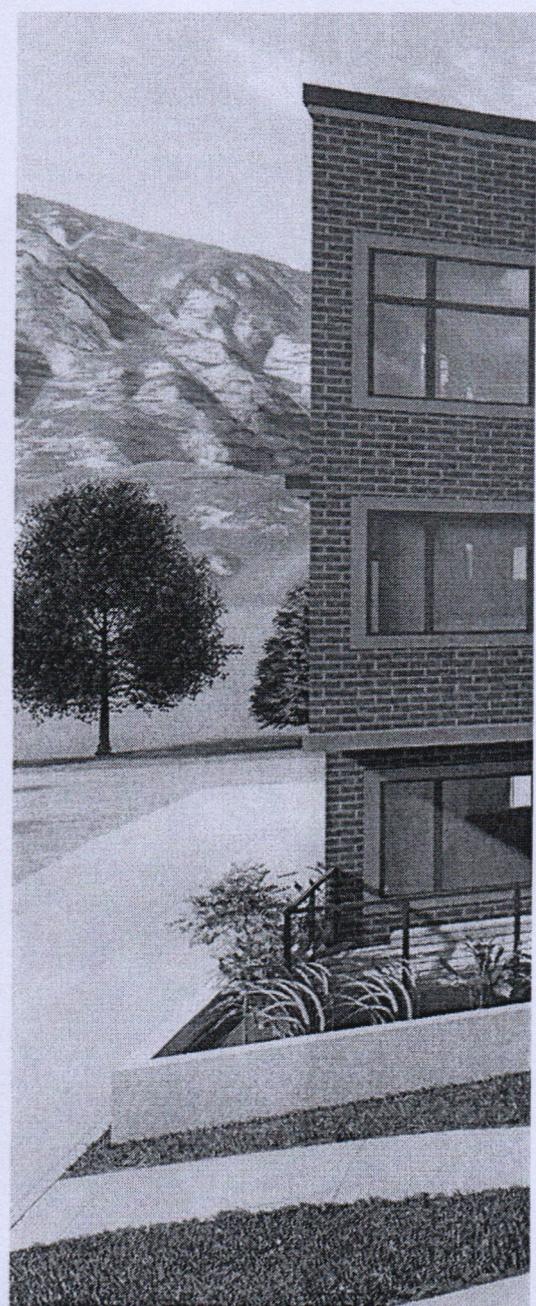
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### **11-3-045: SPECIAL EXCEPTIONS:**

A. Purpose: A special exception is an activity or use incidental to or in addition to a principal use permitted in a zoning district; or an adjustment to a fixed dimension standard permitted as an exception to the requirements of this title; or a transfer of development right (TDR), or rights, established because of blight which results in an additional lot, or lots, or a dwelling unit, or units; or an adaptive reuse of a building or structure eligible, or that may be eligible, for the National Register of Historic Places so long as the adaptive reuse does not compromise such eligibility. A special exception has less potential impact than a conditional use but still requires careful review of such factors as location, design, configuration and/or impacts to determine the desirability of authorizing its establishment on any given site. This section sets forth procedures for considering and approving special exceptions to the provisions of this title.

B. Authority: When expressly provided for under the provisions of this title, the Planning Commission is authorized to approve special exceptions to the provisions of this title in accordance with the terms and provisions set forth in this section.

C. Initiation: A property owner, or the owner's agent, may request a special exception to the provisions of this title in accordance with the procedures set forth herein.

D. Procedure: An application for a special exception shall be considered and processed as follows:

1. A complete application shall be submitted to the Zoning Administrator in a form established by the City along with any fee established by the City's fee schedule. The application shall include at least the following information:

- a. The name, address and telephone number of the applicant and the applicant's agent, if any.
- b. The address and parcel identification of the subject property.
- c. The zone, zone boundaries and present use of the subject property.
- d. A complete description of the proposed special exception.

e. A plot plan showing the following:

- (1) Applicant's name;
- (2) Site address;
- (3) Property boundaries and dimensions;
- (4) Layout of existing and proposed buildings, parking, landscaping and utilities; and
- (5) Adjoining property lines and uses within one hundred feet (100') of the subject property.

f. Such other and further information or documentation as the Zoning Administrator may deem necessary for a full and proper consideration and disposition of a particular application.

2. After the application is determined to be complete, the Zoning Administrator shall schedule a public hearing before the Planning Commission. Notice of public hearings shall be given as required by law and according to policies established by the commission. The Planning Commission shall take action on the application within a reasonable time after the filing of a complete application.

3. A staff report evaluating the application shall be prepared by the Zoning Administrator.

4. The Planning Commission shall hold a public hearing and thereafter shall approve, approve with conditions or deny the application pursuant to the standards set forth in subsection E of this section. Any conditions of approval shall be limited to conditions needed to conform to the special exception to approval standards.

5. After the Planning Commission makes a decision, the Zoning Administrator shall give the applicant written notice of the decision.

6. A record of all special exceptions shall be maintained in the Office of the Zoning Administrator. (Ord. 2018-11, 3-6-2018)

E. Approval Standards: The following standards shall apply to the approval of a special exception:

1. Conditions may be imposed as necessary to prevent or minimize adverse effects upon other property or improvements in the vicinity of the special exception, upon the City as a whole, or upon public facilities and services. These conditions may include, but are not limited to, conditions concerning use, construction, character, location, landscaping, screening, parking and other matters relating to the purposes and objectives of this title. Such conditions shall be expressly set forth in the motion authorizing the special exception.

2. The Planning Commission shall not authorize a special exception unless the evidence presented establishes the proposed special exception:

a. Will not be detrimental to the health, safety or general welfare of persons residing or working in the vicinity, or injurious to property or improvements in the vicinity;

b. Will not create unreasonable traffic hazards;

c. Is located on a lot or parcel of sufficient size to accommodate the special exception.

F. Effect Of Approval: A special exception shall not authorize the establishment of any use nor the development, construction, reconstruction, alteration or moving of any building or structure, but shall merely authorize the preparation, filing and processing of applications for any approvals or permits that may be required by this title or other applicable provisions of this Code.

G. Amendments: The procedure for amending a special exception shall be the same as the original procedure set forth in this section.

H. Expiration: Subject to an extension of time, a special exception which is not exercised within one hundred eighty (180) days shall expire and have no further force or effect. (Ord. 2002-48, 12-11-2002)